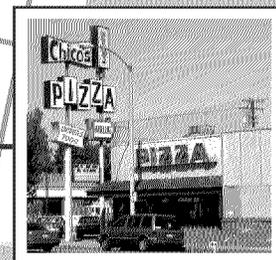
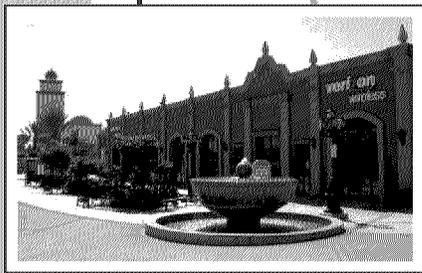
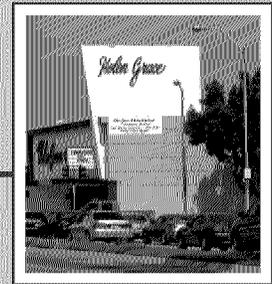
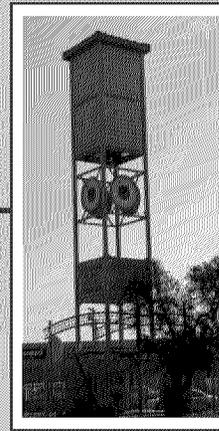
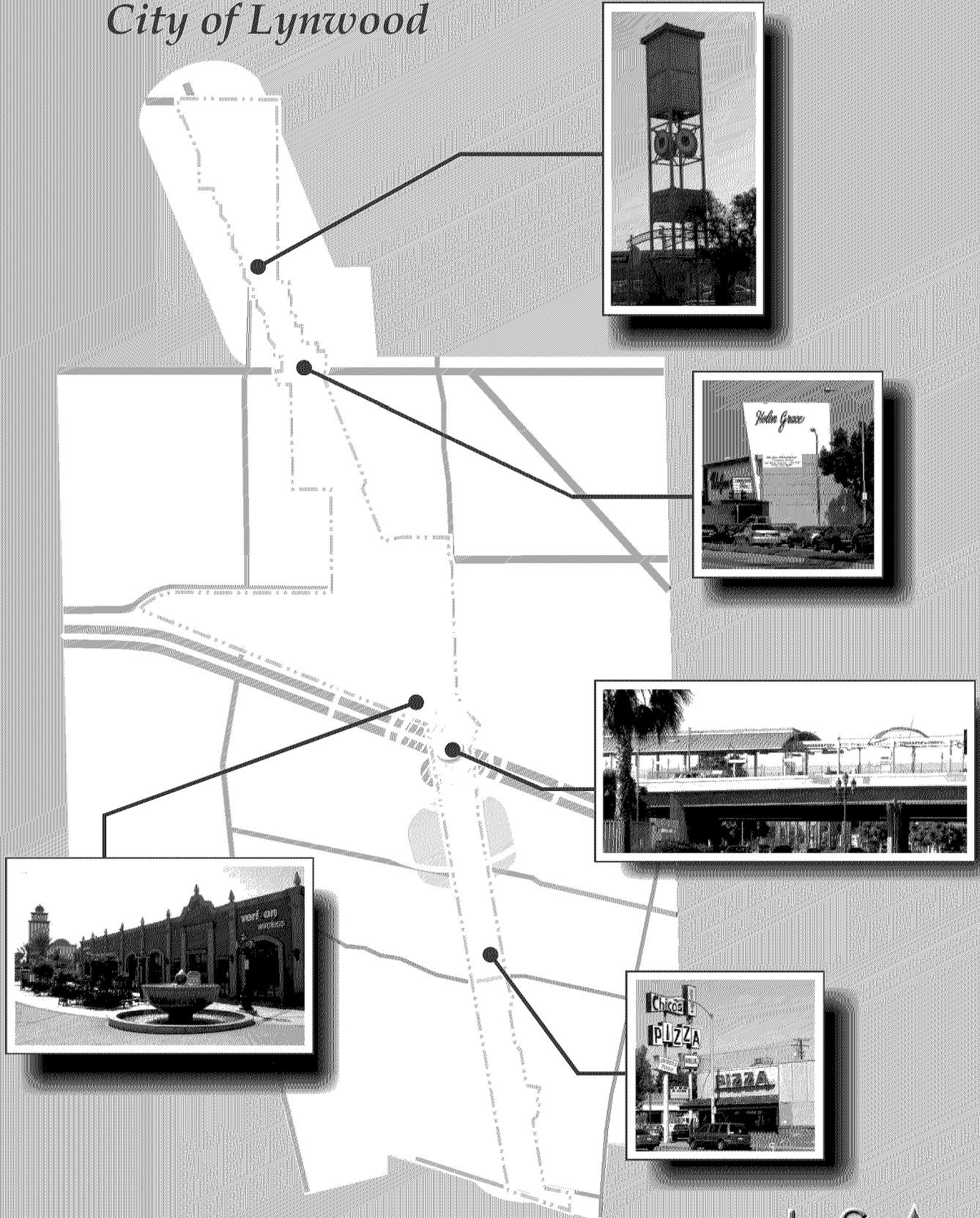


Long Beach Boulevard Specific Plan

City of Lynwood



LONG BEACH BOULEVARD SPECIFIC PLAN

Specific Plan
November 2006

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1.0 INTRODUCTION

1.1 PROJECT BACKGROUND

The City of Lynwood is one of the Gateway Cities in the County of Los Angeles. The City has 73,212 residents residing over the 4.9 square miles area. It is located between two freeways, Interstate 710 and Interstate 105. Long Beach Boulevard is a primary commercial corridor in the City of Lynwood.

Long Beach Boulevard is the major north-south thoroughfare in the County and stretches about 2.3 miles within the City of Lynwood. The Boulevard has been undergoing significant investments; these include the Green Line light rail on Interstate 105 (I-105) in 1995; the 36-acre open air village retail center (Plaza de Mexico) in 2000; Long Beach Pluma Shopping Center and other retail developments along the northern parts. The Boulevard also has several small businesses and ethnic markets. It is characterized by heavy pedestrian and vehicle traffic. Issues of concern along the Boulevard include graffiti, crime, and run-down buildings that are of poor architecture quality in a state of disrepair.

The City has identified a need to set forth a framework to guide all development, with emphasis on design, development standards, and right-of-way improvements along the Boulevard. This Specific Plan focuses on Long Beach Boulevard for this purpose.

1.2 INTENT AND PURPOSE OF THE SPECIFIC PLAN

The project area, shown in Figures 1.1 and 1.2, is part of the Redevelopment project

area in the City of Lynwood. It is poised for major new developments along the Boulevard, particularly in the vicinity of the intersection with I-105.

The objectives for new development in the project area are:

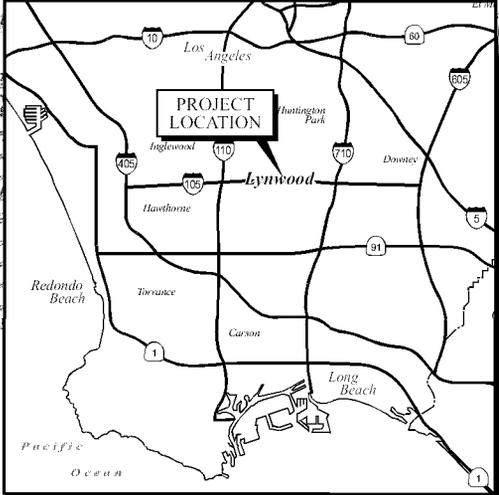
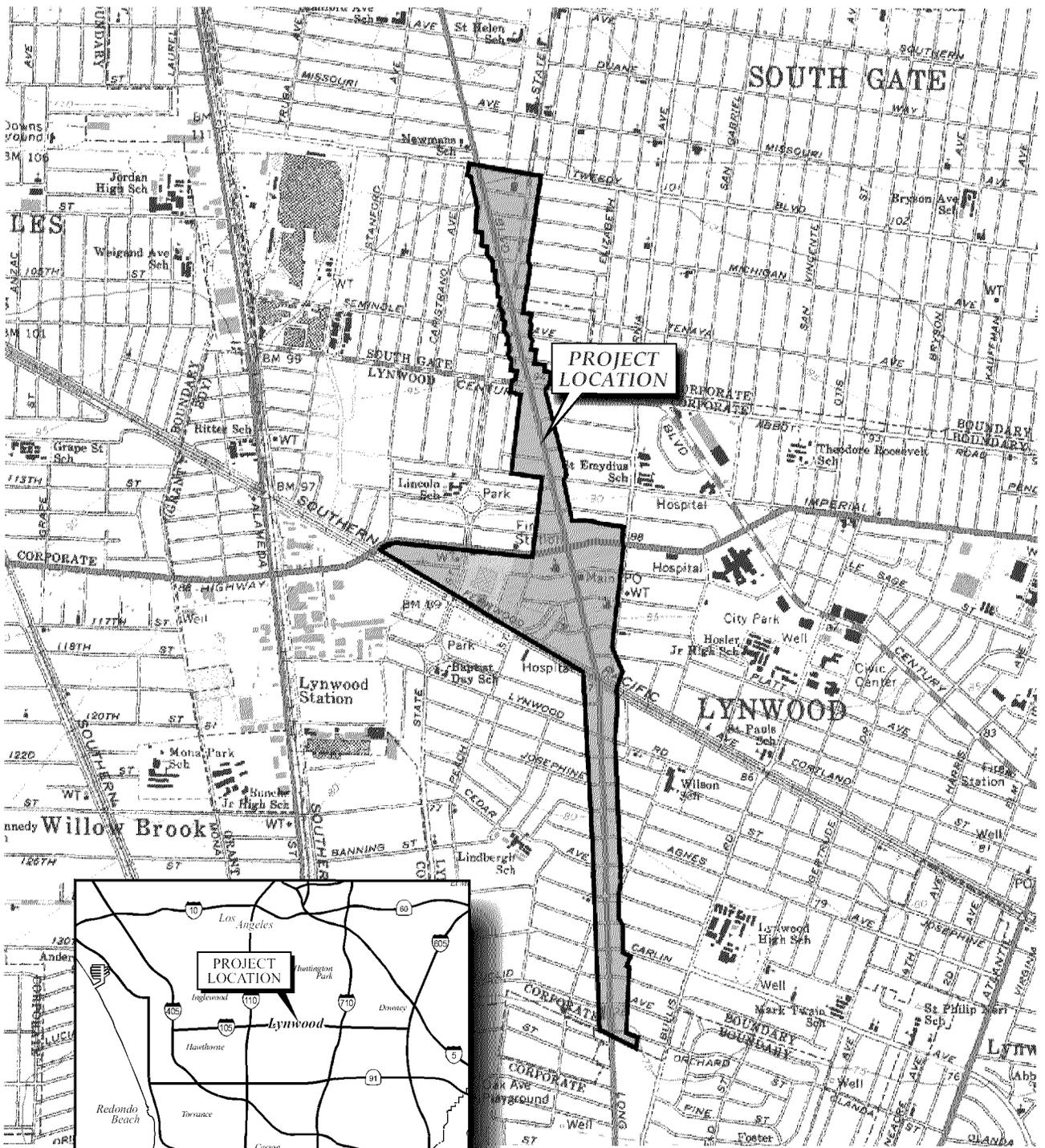
- To revitalize the Boulevard;
- Increase the transit use;
- Instill downtown presence on the Boulevard; and
- Provide a base for economic vitality for the future.

The City's intent is to achieve a high-quality, aesthetically attractive urban area with land uses that are conducive to the presence of transit along I-105 and the numerous small, local markets that are also vital to the community.

This Specific Plan identifies key issues, creates a land use development program based on sound planning principles, and establishes design guidelines for architecture and landscaping along Long Beach Boulevard. The Specific Plan provides direction to investors, developers, City and the Agency by establishing a framework for the visual quality desired by the City for establishing downtown presence along Long Beach Boulevard. The land use standards are set with a level of flexibility to address the changing demands of the real estate market.

Long Beach Boulevard Specific Plan

Figure 1.1: Project Location

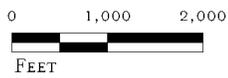


NOT TO SCALE

- LSA
-  REVITALIZATION AREA PROJECT BOUNDARY

Long Beach Boulevard Specific Plan

Figure 1.2: Project Location Aerial



1.3 AUTHORITY AND SCOPE

Government Code Section 65450, et seq. authorizes cities and counties to adopt specific plans to implement their general plans, which include text and graphics addressing the following:

- The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan.
- The proposed distribution, location, and extent and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area and needed to support the land uses described in the plan.
- Any and all development and design standards, including criteria for architectural, streetscape and landscape design.
- A program of implementation measures including regulations, programs, public works projects, and financing measures to implement all of the above.
- The relationship of the specific plan to the general plan.

In addition, the specific plan may address other subjects that, in the judgment of the planning agency, are necessary or desirable for implementation of the general plan. State law permits a specific plan to be prepared, adopted, or amended in the same manner as a general plan, except that a specific plan may be adopted by resolution or by ordinance, and may be amended as often as is deemed necessary

by the legislative body and must be consistent with a city's general plan.

1.4 CONSISTENCY WITH GENERAL PLAN GOALS, OBJECTIVES, AND POLICIES

Specific plans must be compatible with goals and policies of the adopted General Plans of local jurisdictions (California Government Code Section 65454). In this case, the City of Lynwood General Plan (adopted in 2003) is the governing document.

The City of Lynwood's General Plan designates the Long Beach Boulevard Redevelopment Project area "A" with commercial, multi-family, single family, public facility and Specific Plan Area land uses.

The Long Beach Boulevard Specific Plan is consistent with the goals of the General Plan, while allowing implementing development solutions in the project area.

The Specific Plan pays special attention to the following General Plan goals and policies:

1.4.1 COMMUNITY DESIGN ELEMENT

The Community Design Element envisions new development within the Specific Plan area to be attractive, safe, well-designed, and well-integrated with adjacent neighborhoods, while identifying proper corridors, gateways, and nodes. Such identified corridors will present unique opportunities for enhancing the City's urban design image.

Goal No. 1. Encourage physical development that enhances the positive image of the City as a balanced residential community.

Goal No. 2. Define urban design components that provide a unique visual character for the City and distinguish the City from its neighboring communities.

Goal No. 3. Develop innovative strategies for bridging the physical separation of the City created by I-105 such as, but not limited to, continuous and consistent sidewalk design, lighting for safety, and art.

In the process of developing generalized urban design goals for the City of Lynwood, specific policies were formulated to further refine and provide substantive recommendations in support of the community design goals articulated above. The policies, which accompany these goals, are as follows:

Policy No. 1: Formulate general design guidelines for residential and commercial properties to ensure that new construction and renovation of existing structures achieve a high level of architectural and site design quality.

Policy No. 2: Develop design guidelines, which facilitate the creation and identification of distinct neighborhoods throughout the City.

Policy No. 3: Develop street median landscape standards to enhance the streetscape.

Policy No. 4: Ensure that signage is visually attractive, compatible within the neighborhood setting and provides a high quality image for the City.

1.4.2 ECONOMIC ELEMENT

The Specific Plan correlates with the General Plan to strengthen the existing businesses along Long Beach Boulevard through the use of revitalization and programs such as, but not limited to, Community Development Block Grant (CDBG) incentives. Tools to enhance the local market include infrastructure improvements, balanced accessibility for vehicles, bicycles, and pedestrians, appropriate façade treatments, streetscapes, and proper signage. Both plans share the goal of eliminating blight and enhancing the economic and social well being of Lynwood. The Specific Plan complies with the General Plan's goals and policies as follows:

Policy ED-1.2: Local Economic Development. The community shall strive to diversify its local business makeup so that it avoids dependence on one segment of the local economy to provide employment, revenues, and retail outlets for the citizenry.

1.4.3 CIRCULATION ELEMENT

The Circulation Element of the City's General Plan addresses the City's plan to widen certain streets, relocate some parking, and provide a system of pedestrian walkways and bike lanes. The Specific Plan will provide for an attractive well-connected street and pedestrian system that accommodates the needs of all users, including pedestrians and cyclists, and strengthens the access to the Green Line light rail on I-105. The Specific Plan focuses on following goals and policies from the General Plan:

Policy CIR-1.5: Pedestrian-Friendly Systems. Establish street sections in residential neighborhoods and certain commercial areas that, while accommodating vehicular traffic, give preference to pedestrian users.

Goal CIR-2. Accommodate alternatives to private automobile transportation that meet the need of all City residents.

Goal CIR-3. Promote a regional transportation system that serves existing and future travel between Lynwood and other population and employment centers within the region.

Goal CIR-4. Provide an adequate supply of private off-street and public parking to meet the needs of residents and visitors to the City.

Policy CIR-4.2: Public/Private Parking Facilities. Provide joint-use and public parking facilities where needed by special assessment districts or other mechanisms.

1.4.4 LAND USE ELEMENT

The Land Use Element of the Specific Plan provides a range of land uses to accommodate the living, working, shopping and recreational needs of the City's growing population with a diversity of uses that will promote economic growth. Land uses also take advantage of the Specific Plan area's unique condition of being well-served by regional transit systems and the mixed-use nature of the area. The Specific Plan complies with the General Plan's goals and policies as follows:

Goal LU-2: To plan for a range of commercial sites within the Planning Area

to serve the needs of those living, working, and visiting Lynwood. These commercial areas will provide a range of commercial opportunities in line with the needs of the above groups, and will continue to develop Lynwood as a retail center for the area.

Goal LU-6: Provide a framework that could encourage the combination of commercial, medium/high density residential, and active and passive open space uses within a specific area to create a vibrant village atmosphere dominated by pedestrian-oriented land uses.

1.4.5 INFRASTRUCTURE AND PUBLIC SERVICES

The Specific Plan complies with the City General Plan's goal as follows:

Goal DW-1: Provide for the planning and funding mechanism to construct, expand, and maintain water facilities (transmission, storage, distribution, and treatment) needed to meet current and future demand.

As the market conditions have changed, a high amount of development is on the forefront for the City's near future. This Specific Plan will provide the City the opportunity to develop a logical mix of uses at a manageable scale and design quality suited for the community while capitalizing on the demand for more commercial uses.

The City of Lynwood designates the project area Specific Plan, and designates the Long Beach Boulevard Specific Plan as the adopted Specific Plan.

1.5 RELATIONSHIP TO REDEVELOPMENT PLAN

The City of Lynwood adopted the amended Redevelopment Plan in 1998. The Specific Plan follows the boundaries of the Redevelopment Project area "A." To accommodate residential development proposed at that time and to maintain the direction for development of commercial uses, the Redevelopment Project Area "A" Plan was amended to expand the project boundaries, include specific land uses, limitations on type, size, and height of the buildings, open space, provide the legal description of the project boundary, and the Proposed Public Improvement/Projects List.

As market conditions and quality of land uses have evolved over the last few decades, so has the City's direction for this site. The Specific Plan Area provides the City the opportunity to develop a favorable and flexible mix of uses at a manageable scale and design quality desired by the market and community. To further that mission, this Specific Plan will remain compatible with future goals and provisions outlined in the amended Redevelopment Plan for Redevelopment Project "A."

The direction set forth in this Specific Plan concurs with the objectives set forth in the amended Redevelopment Plan.

The purpose of this Specific Plan is to provide the energy and authority for the highly anticipated revitalization and upgrade of the Long Beach Boulevard corridor. The primary policies set forth in this Specific Plan are as follows:

1. *Initiate specified land use changes in the area and eliminate unfavorable land uses by:*
 - Reducing the number of parcels in the Project Area by possible land assembly and re-subdivision of land area.
 - Utilizing long-term planning and agency resources.
 - Conforming to the Redevelopment Plan and the implementation plans of the Agency.
 - Providing a relationship of proposed developments to the surrounding community, quality, configuration, appearance, and servicing the needs of the community that will enhance the City's integrity.
 - Contribution of the proposed development to the tax base of the community.
2. *Permit commercial uses that include, but are not limited to, neighborhood commercial, highway commercial, general commercial, and supporting vehicle parking facilities.*
 - Other land uses permitted within the Project boundaries are industrial park and manufacturing, public uses, including semi-public, institutional, and nonprofit uses. The land coverage permitted is as specified in the zoning ordinance.
3. *Enhance the open space and intersperse with other uses in any area by maintenance, establishment, and/or enlargement of such land use.*
 - The approximate amount of open space to be provided in the project area expansion is the total of all areas that will be in the public

rights-of-way, the public ground, the space around buildings, and all other outdoor areas not permitted though limits on land covered by this Plan to be covered by buildings and are permitted under the City's General Plan.

4. *Propose public improvement projects for arterials, sidewalks, lighting, utilities, and landscape.*

- The Redevelopment Plan authorizes installation, construction, reconstruction, redesign, or reuse of streets, sidewalks, gutters, parking lots, bridges, streetlights, signalization on streets, water distribution system, natural gas, electrical, and telephone distribution systems, parks, plazas, playgrounds, and other public landscaped areas.
- Arterial improvements include widening, resurfacing, improved signalization, and channelization. General improvements to sidewalks, lighting, under-grounding of utilities.
- Street and alleys may be widened, altered, abandoned, or closed as necessary for property development in the project area.
- Proposed and new development shall provide a balance between adequate pedestrian and vehicular access, vehicular parking, and delivery loading docks with similar needs of existing developments.

1.6 PRINCIPLES OF PLANNING

The Specific Plan land use planning program is developed for the purpose of

achieving all the objectives highlighted earlier.

In order to maintain and increase the vitality of the corridor and its importance to the City, the following principles of planning are implemented in this Plan.

- Transit-Oriented Development ;
 - Mixed uses,
 - Pedestrian connections, and
 - Multiple housing;
- Live-work housing; and
- Small business retention.

Small business retention is key to an area that is characterized by many local businesses. Many of these businesses epitomize the investment and innovation of the local residents for products and services that are of value to the community. The fine-grained nature of the local economy is further reinforced in the development of retail and commercial in all parts of the project area.

Based on the existing diversity, density, and Green Line light rail, transit-oriented development is another one of the fundamental principles for planning along the Boulevard. Transit-Oriented Development (TOD) along Long Beach Boulevard will generate the mix of uses and density to meet the needs of the community while leveraging the transit use.

1.6.1 TRANSIT-ORIENTED DEVELOPMENT

TOD refers to any community that efficiently relies on bus or rail transit systems as its primary source of travel.

Multiple-family housing and mixed-use projects creating an environment safe for pedestrians are primary hallmarks of TOD.

It involves higher densities, reduced parking requirements, planned pedestrian walkways, a pedestrian-accessible transit station, a variety of services, and buildings oriented toward street activity. TOD enables planning at a single site location as well as at a regional level and arranges land uses that support the transit system.

According to the California Department of Transportation (Caltrans) Study, "Factors for Success in California" (May 2002), TOD has been described as "moderate to higher density development, located within an easy walk of a major transit stop, generally with a mix of residential, employment and shopping opportunities designed for pedestrians without excluding the auto. TOD can be new construction or redevelopment of one or more buildings whose design and orientation facilitate transit use." TOD is not limited to large or small communities and can be accommodated in several different settings.

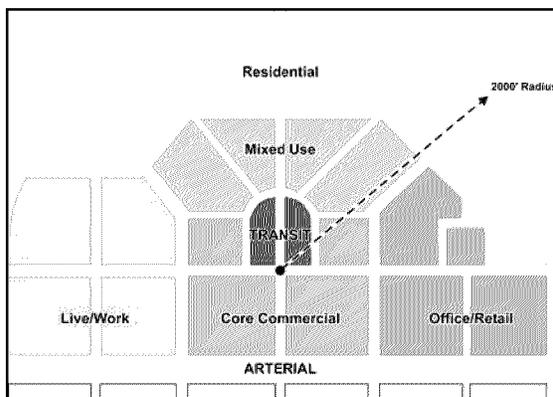


Figure 1.3: TOD Diagram

It is expected that California will continue to adopt TOD principles in several of its

existing communities over the next 20 years as a result of the increase in population, increased demand for additional housing around public transit, increase in traffic congestion, and the desire for walkable districts. Adoption of TOD principles are already in place in surrounding jurisdictions, including, but not limited to, Los Angeles, San Diego, Pasadena, and Glendale.

1.6.2 BENEFITS OF TOD

The beneficial effects of TOD range from individual to regional scales. Typical design features of TOD have, in several existing cases, improved neighborhood commercial quality, increased retail activity, increased sale tax revenues, increased transit ridership, reduced crime, enhanced public safety, and increased property values. In terms of housing opportunities, the adoption of higher densities in a TOD district creates the potential for diverse housing types and affordability.

TOD offers other economic benefits for the community such as direct financial savings that would accrue to taxpayers and commuters from a more efficient use of a working transportation system, thereby increasing household disposable income through the following factors:

Reduced auto ownership. TOD and smart land use planning can give twice as many households access to frequent transit service compared to the status quo, increasing transit patronage. As a result, Lynwood residents would own fewer cars, resulting in huge cost savings, since car ownership is the main reason that transportation spending is the second-largest expense for California households

after housing. The American Automobile Association estimates that the cost of owning a car is \$7,456 (excluding fuel costs, which are covered in the next paragraph). If only 30 percent of the City's workforce population used the transit system as its primary means of transportation, *the combined financial savings to these residents would be \$53.9 million per year.*

Reduced gasoline costs. The lower levels of driving that can be achieved with TOD go beyond just helping to reduce congestion. It also reduces the amount of gasoline that Lynwood residents would have to buy. Households with limited income can be negatively impacted by gasoline price increases, which are approximately 4 percent, according to www.transact.org. Working families making less than the national median income tend to be hardest hit by rising gas prices.

A two or more person household that owns one car and does not use transit spends an average of \$1,311 on gasoline a year. That same household, if its members are heavy transit users, spends an average of \$609 on gasoline a year; a savings of \$702 annually (www.transact.org).

In a TOD district, Lynwood residents would drive significantly fewer miles, save money on gasoline, and save wear and tear on their vehicles. Drivers in the Los Angeles area waste over half a gallon of gas a day due to traffic congestion (www.bestplaces.net). In the Los Angeles area, 18.4 percent (\$9,162) of household expenditures is spent on transportation (www.transact.org).

Small increase in spending on transit fares. An estimate of how much more

money Lynwood residents would spend on transit fares is needed to capture fully the direct transportation costs of a TOD future. Not all of this would necessarily be an additional cost. When transit agencies need additional revenue, they often seek additional taxpayer funding. Greater fare revenue reduces the need for operating subsidies.

In addition, TOD improves the quality of the living environment by reducing traffic congestion and commute times, thereby improving air and water quality and providing walkable neighborhoods for residents and consumer activity. Table 1.A summarizes the benefits of TOD and characterizes the public and private impacts.

1.6.3 TOD CONCERNS

Although TOD may have several beneficial outcomes, there are also concerns related to planning for its success. Typical concerns include the design of the transit system and how it relates to the surrounding community; local community concerns; compatibility with surrounding zone designations; perceived high cost for developing TOD districts; and limitations in public financing. Such concerns, if not addressed, can cause the failure of a transit district. In the City of Lynwood, concerns include the surrounding zones adjacent to the TOD district, as well as the high costs involved in development of the TOD.

Adjacent to the Specific Plan area are existing single-family and multifamily residential uses whose concerns include the impacts of an increase in density, noise, and activity. A further concern is

Table 1.A – Benefits of Transit-Oriented Development

Public	Private
<ul style="list-style-type: none"> • Revitalize declining communities • Create opportunity for diverse housing • Increase use of transit system • Decrease traffic congestion • Improve commute times • Improve environmental and health concerns, such as air and water quality • Sustain a revenue source for transit agencies • Produce sales tax revenues • Create positive local and regional communication • Reduce crime • Decrease time spent in autos • Decrease expenditures spent on roadway services • Increase public safety 	<ul style="list-style-type: none"> • Can increase household disposable income • Increase property values • Improve commercial sales and activity • Improve foot traffic for retailers • Decrease transportation/commuting costs for residents and workers • Provide access to a more diverse workforce • Improvements for pedestrians and transit riders do not come at the expense of automobile access • Co-location of employment with other uses (such as daycare) increases the attractiveness of workplace to prospective employees • Can provide mobility choices • Improves the overall physical health of the community and its residents

providing consistency and harmony between the proposed and existing uses.

- Convincing investors that TOD is an asset class.

1.6.4 MAJOR CHALLENGES TO CREATING HIGH-PERFORMING TOD

- Finding a common definition or agreement on the goals and outcomes;
- Balancing the tension between the requirements of making a project a successful place and making a successful transportation node;
- Reducing complexity, time, uncertainty, and costs;
- Creating a supportive regulatory and policy environment;
- Acknowledging that more than transit is needed to drive real estate investments; and

1.6.5 MAJOR REASONS FOR TOD FAILURES

- A land use focus on only office and commercial uses;
- Lacking the pedestrian connectivity that makes riding transit an easy and appealing alternative;
- Lack of “placemaking” elements, such as plazas and streetscape improvements; and
- Failure of the transit system to provide the regional connectivity that would make it a viable transportation option for residents.

The Los Angeles area ranks highest in the country for the average number of residents per transit zone at 6,557.

Table 1.B - Journey to Work (Percentage of Employed Population in Transit Zones)

Metropolitan Area	Workers who Ride ALL Modes of Transit	Workers who Ride the Rail System	Workers who Ride the Bus	Transit Riders who Walk to Stations	Existing Stations
Washington	30%	22%	8%	10%	169
Chicago	25%	14%	11%	6%	418
Los Angeles	16%	2%	14%	5%	124
Cleveland	13%	3%	10%	6%	50

Source: *Hidden in Plain Sight: Capturing The Demand for Housing Near Transit*, April 2005.

However, the transit system provides some of the poorest coverage with only 0.8 station per 100,000 residents. Although the transit system in Los Angeles is small relative to the size of the overall region, its transit zones are densely inhabited and, therefore, have the potential to serve a significant number of people. The density in the City of Lynwood is 14,941 residents per square mile. Due to the limited fixed rail system in the Los Angeles area, a smaller percentage of people use the rail transit compared to other metro areas with transit, even if they live close to a station as seen in Table 1.B. This explains why density alone is not enough for a successful TOD district; the system also needs regional connectivity and a sufficient number of transit stations.



Green Line on I-105 perpendicular to Long Beach Boulevard

Only 16 percent of residents of transit zones in the Los Angeles area ride all modes of transit to work. Of that number, 14 percent are using buses; therefore, the rail system receives only a small share of the transit trips.

1.6.6 MIXED USES

Mixed uses within the Specific Plan include first-floor commercial or retail activity and upper floors of residential units. Mixed uses within TOD districts provide a consistent form of activity and utilize transit system operations. The mixed-use component creates an opportunity for diverse housing and living environments. Pedestrian-oriented streetscapes will enhance the commercial corridor creating a friendly and inviting atmosphere. Residents located within the mixed-use units will have easy access to the transit system and ground-floor commercial uses.

1.6.7 LIVE AND WORK

TOD districts include a variety of services and land uses. Live and work housing is a type of development that provides an environment where an individual can occupy a residential unit within the same building as his or her work, where

walking is an option, and where public transportation is the primary choice. Typical neighborhoods include pedestrian-friendly walkways, walking access to adjacent districts, pedestrian safety, foot traffic, and local and regional public transit.

1.6.8 STEPS TOWARD IMPLEMENTATION

The idea of building around a transit system is slowly emerging in the United States. The desire to live near a transit system has become appealing to many commuters tired of congested traffic and long hours of travel. Implementation of TOD enables individuals to choose a type of travel and provides a variety of mobility. In addition, recognizing that development around transit systems requires planning for success and sustainability. Table 1.C summarizes the basic steps to creating a successful TOD district. Additional measures to ensure the success of TOD include:

Limit Development Type for TOD. Within a ¼ mile walk of a transit station, create development that generates and supports ridership, and avoid big-box retailers.

Appropriate Residential Densities for TOD. According to the study, *The New Transit Town: Best Practices for Transit-Oriented Development*, the TOD type “Urban Neighborhood” should have a minimum housing density of 20 units per acre. The “Urban Downtown” should have a minimum housing density of 60 units per acre.

The current urban characteristics of Lynwood are somewhere in between these two TOD types. Currently, the residential density for Lynwood is 4.2 for the transit zone (see Table 1.D). The Lynwood Zoning Code currently allows for a maximum of 18 units per acre. The study, *Hidden in Plain Sight: Capturing the Demand for Housing near Transit* suggests that “demand for higher-density housing in transit zones (in this case, the Los Angeles area) could far outstrip the supply of this kind of housing.” There is a clear need to increase the number of housing units and maximum densities within the transit zone to accommodate the market demand for housing. Thus, the land use plan proposes densities in mixed uses.

Table 1.C - Planning for TOD Success

• Achieve a vision rather than a plan
• Create a destination rather than a project
• Optimal Transit System Design
• Promote and encourage transit ridership
• Create Community Partnership
• Design retail development market-driven as opposed to transit driven
• Adopt mixed uses and live/work districts
• Provide incentives for individuals to live around transit
• Have efficient parking through shared parking and reduced parking incentives; set maximum parking limits for residential developments rather than minimum parking requirements
• Plan for bicycle lanes within a one-mile radius of transit stations

Table 1.C - Planning for TOD Success

<ul style="list-style-type: none"> • Coordination between local regional and State organizations • Provide financial and other incentives • Encourage commercial attention

Table 1.D - Transit Zones Characteristics

	Lynwood Transit Zone*	Typical Urban Neighborhood	Typical Urban Downtown
Average Miles to Major City	5.0	6.0	5.0
Average Residential Density	4.2	8.5	31.4
Average Block Size (Acres)	1.6	7.2	3.9
Average Year Housing Built	1954	1954	1950

* Includes Census Tract 5402.03 and 5405.01

2.0 EXISTING SITE CONDITIONS

2.1 SITE AREA DESCRIPTION

The Long Beach Boulevard Specific Plan area is a linear, 2.37-mile, north-south aligned project site covering approximately 140 acres in the City of Lynwood. It is one of the major transportation corridors in Lynwood and includes blocks facing Long Beach Boulevard between Tweedy Boulevard to the north and Orchard Street to the south. It is generally one block deep as seen in Figure 2.1. Typical of major corridors of urban cities, Long Beach Boulevard is busy with vehicular traffic. There is also significant pedestrian movement.

Long Beach Boulevard intersects Century Freeway (I-105) as seen in Figure 2.1. The Metro "Green Line" runs along the center of I-105 and across and above grade over Long Beach Boulevard.

The project site has buildings ranging in age from the 1930s to some current construction. Most of the adjacent buildings are retail-commercial, motels, restaurants, strip malls, and a large retail center that is fairly new. Single-family homes and multiple-family homes are scattered among other uses. There are few public facility uses and one elementary (public) school in the southern part of the project area.

Long Beach Boulevard is full of people and is active most times of the day. Mom-and-Pop stores have given way to ethnic markets, mainly Latino markets. Graffiti is highly visible indicating some gang activity. Sidewalks are busy and not consistent through the corridor. Other issues seen in the Redevelopment Project

area are crime, prostitution, and lack of adequate parking. These issues are more acute along the southern part of the Boulevard.

Despite these problems, major developments such as Plaza Mexico are harbingers of the changing real estate markets. Long Beach Boulevard is poised to see several new developments that will involve tear-down and rebuild with new businesses.



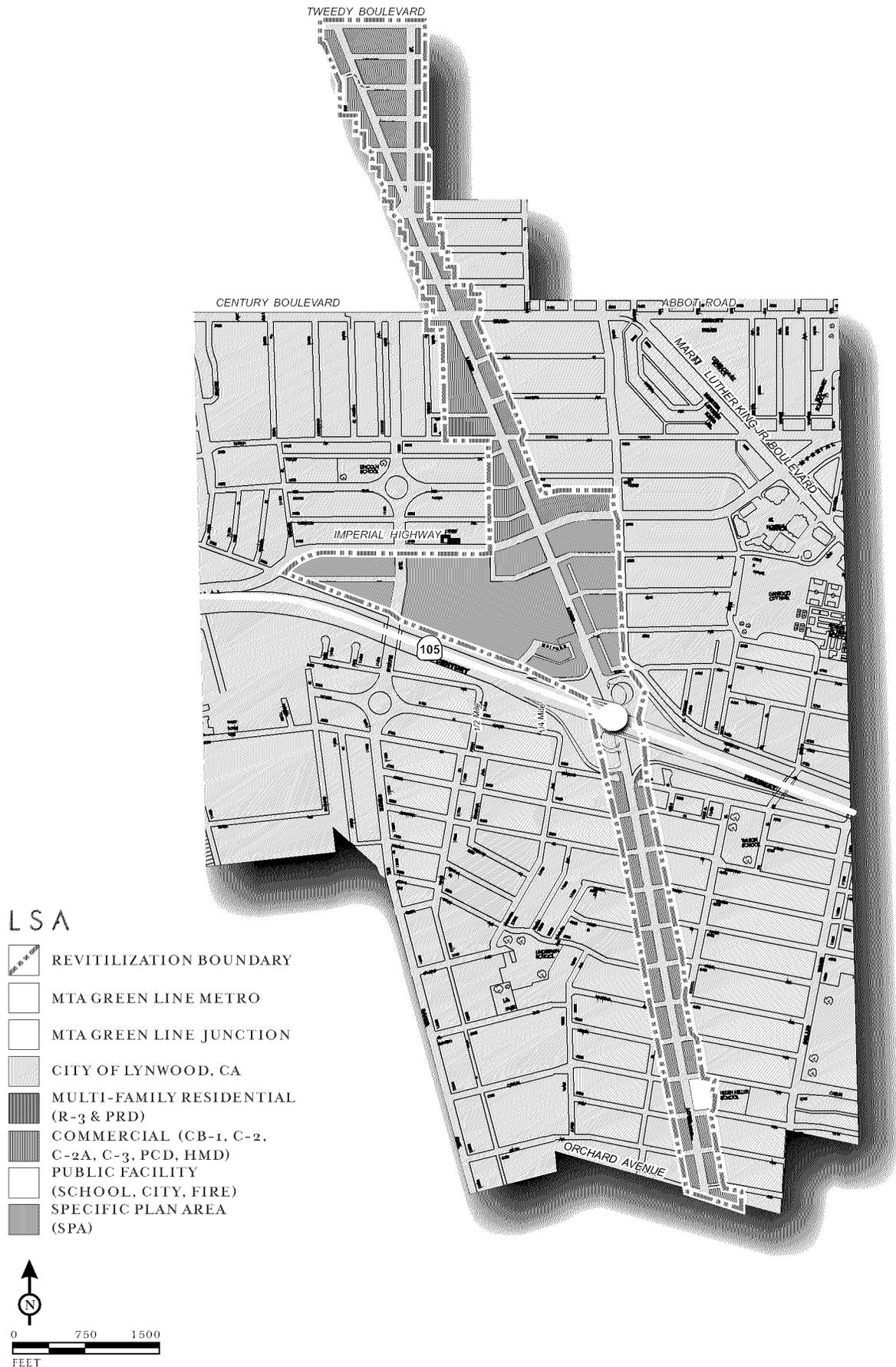
Plaza Mexico (Source: City of Lynwood website)

2.2 CURRENT LAND USE AND ZONING

A dataset of 556 parcels was analyzed for the current land uses and zoning. This detailed parcel dataset is provided in Appendix B.

Long Beach Boulevard Specific Plan

Figure 2.1: Current Land Use



2.2.1 CURRENT LAND USE DESIGNATION

According to parcel data supplied by the City of Lynwood and Los Angeles County, the Long Beach Boulevard Specific Plan project area contains a diverse number of land uses as seen in Table 2.A and Figure 2.1-Current Land Use.

Table 2.A - Current Land Use Designation

Land Use	Acres	% of Total Acres
Single-Family	0.537	0.4
Multifamily	17.565	12.7
Commercial	109.889	79.6
Public Facility	4.293	3.1
Industrial	6.220	4.5
TOTAL	137.967	100.0

Commercial uses along Long Beach Boulevard account for 390 parcels or 70 percent of the total number in the project area. Multifamily residential is the second largest land use, with 115 parcels and covering 17.5 acres. Under the City's 2004 General Plan Map, the Specific Plan Area (SPA) covers 116 parcels located north of I-105 of which 59 parcels are currently residential and commercial/industrial uses.

The block of parcels between Carlin Avenue and Palm Avenue, along Long Beach Boulevard has a General Plan Map Land Use for "Public Facility," with a recently constructed public elementary school on 4.3 acres.

Four detail land use plans shown in Figures 2.3 through 2.5 start at north of the project site and progress through the Boulevard. They illustrate the current land uses and a summary.

2.2.2 ZONING DESIGNATION

According to parcel data supplied by the City of Lynwood, the project area's zoning designations are listed below in Table 2.B. Figure 2.6-Current Zoning Plan illustrates the zoning over the project area.

Table 2.B - Zoning Designations

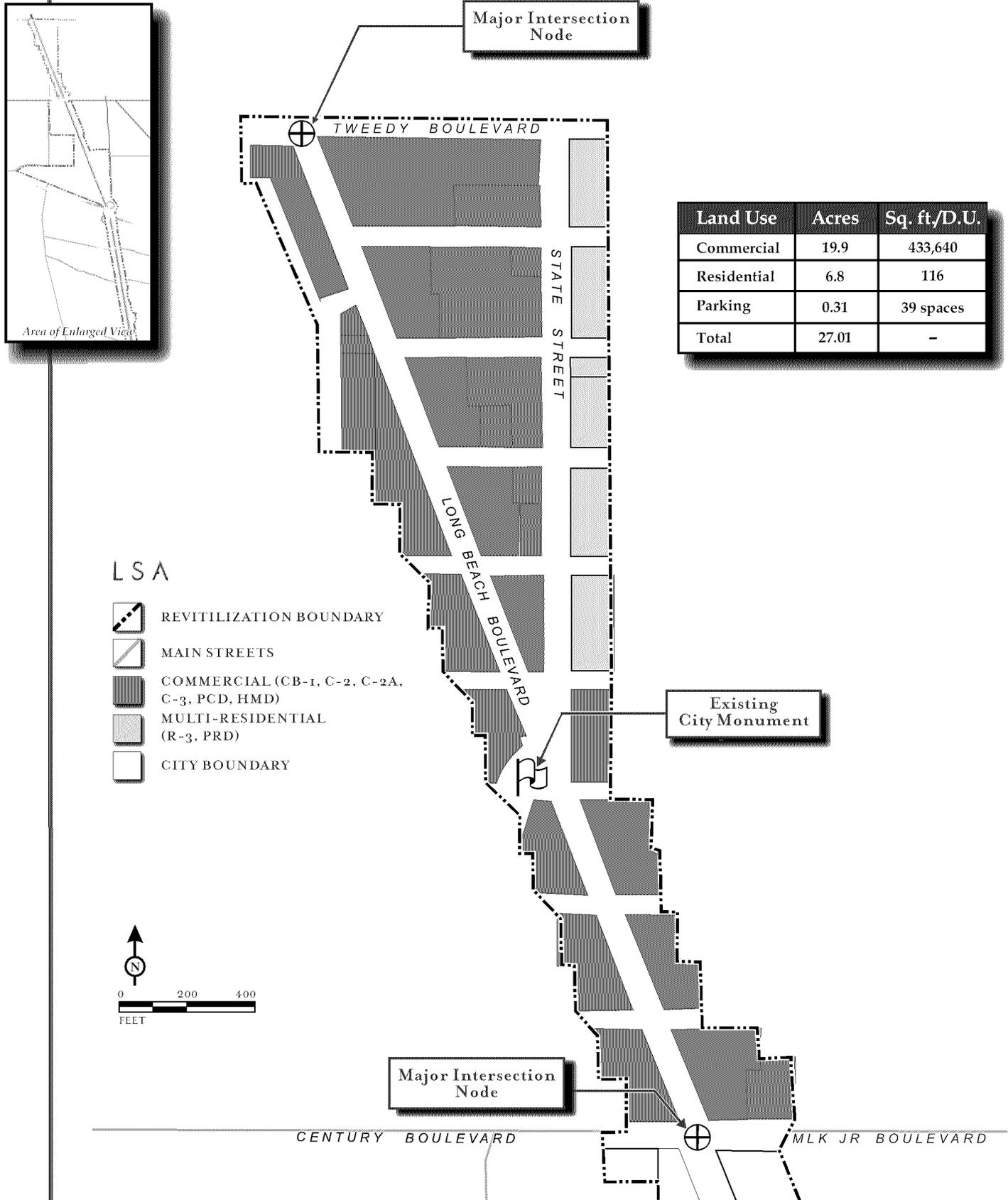
Zoning Designation	Acres	%
R-1: Single-Family	0.537	0.3
R-3: Multifamily	17.565	12.7
P-1: Parking	4.967	3.6
CB-1: Planned Business	32.662	23.7
C-2: Light Commercial	11.249	8.2
C-2A: Medium Commercial	44.324	32.1
C-3: Heavy Commercial	20.443	14.8
M: Manufacturing	6.220	4.5
TOTAL	137.967	100.0

- *Commercial Zones*

- **Zone CB-1:** Planned Business is mostly located in the Lynwood Towne Center, and one block at the corner of Long Beach Boulevard and Orchard Avenue, accounting for 70 parcels or 12.6 percent of the total number in the planning area. The Planned Business (CB-1) district provides locations where daytime, service-oriented businesses may locate to allow for the efficient interaction of complementary service industries. Professional offices, including medical offices and clinics, represent the primary permitted uses. Minor support uses may include small restaurants, cafes, retail shops, and child daycare centers.

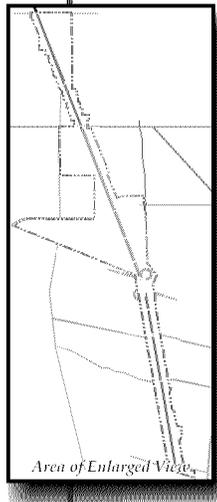
Long Beach Boulevard Specific Plan

Figure 2.2: Existing Downtown Village I Land Use Plan

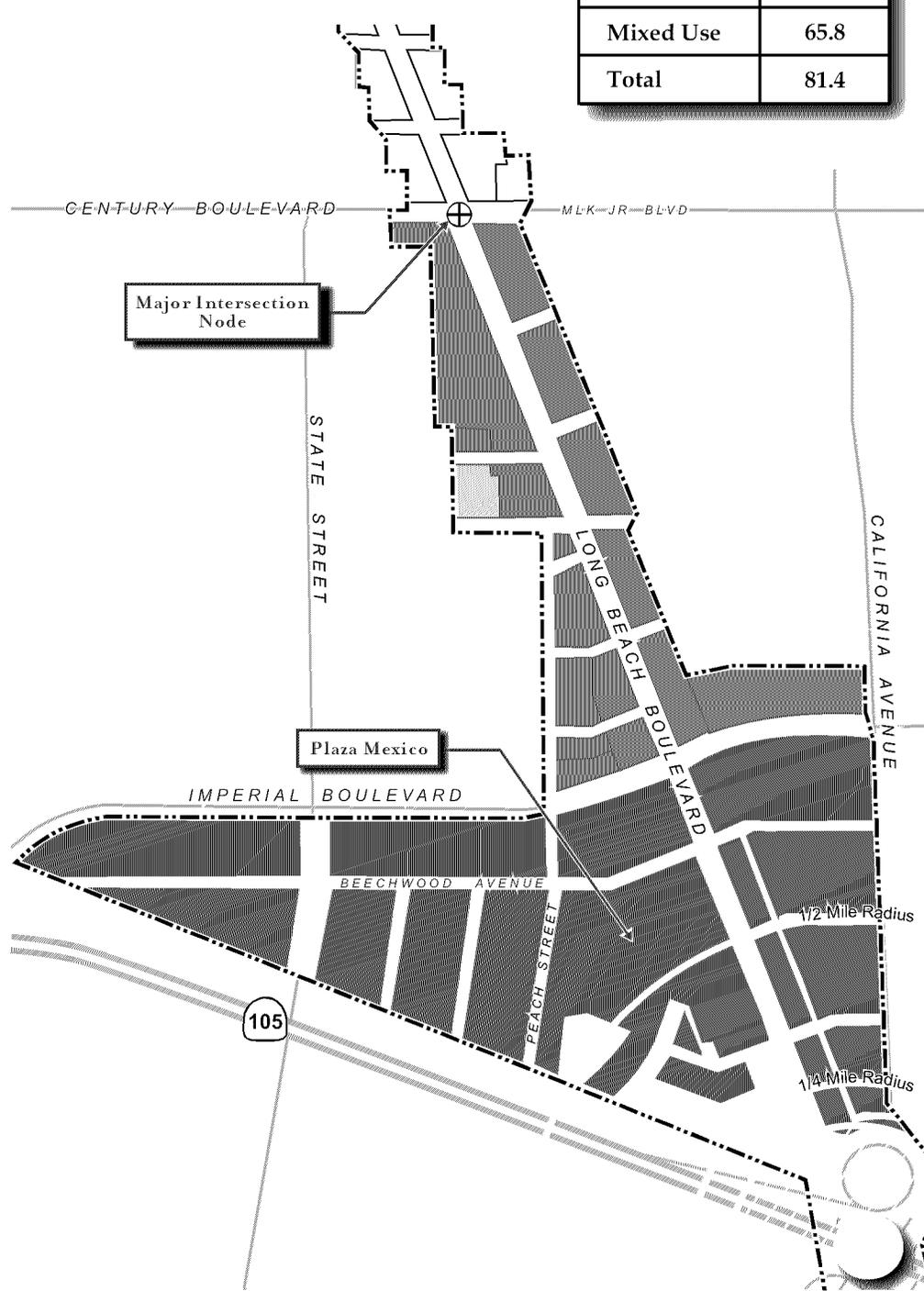


Long Beach Boulevard Specific Plan

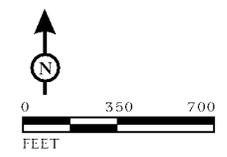
Figure 2.3: Existing Downtown Village II Land Use Plan



Land Use	Acres
Commercial	15.6
Mixed Use	65.8
Total	81.4

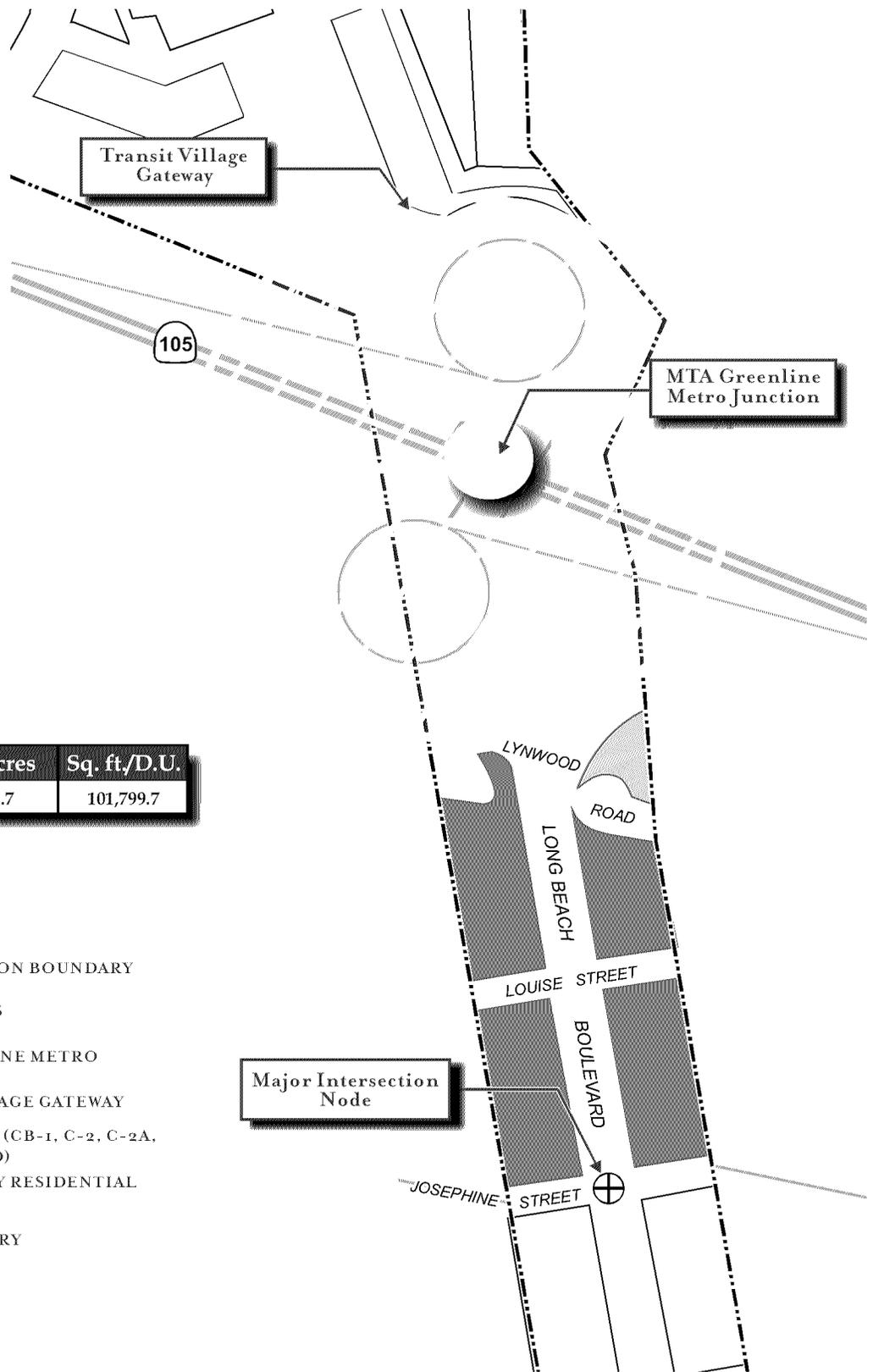
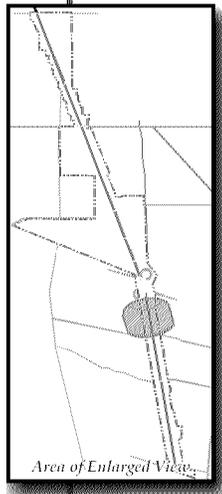


- LSA**
- REVITALIZATION BOUNDARY
 - MAIN STREETS
 - MTA GREEN LINE METRO
 - TRANSIT VILLAGE GATEWAY
 - COMMERCIAL (CB-1, C-2, C-2A, C-3, PCD, HMD)
 - SPECIFIC PLAN AREA (SPA)
 - MULTI-FAMILY RESIDENTIAL (R-3 & PRD)
 - CITY BOUNDARY



Long Beach Boulevard Specific Plan

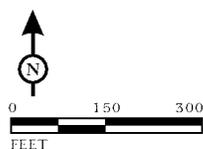
Figure 2.4: Existing Village III-Transit Village Land Use Plan



Land Use	Acres	Sq. ft./D.U.
Commercial	4.7	101,799.7

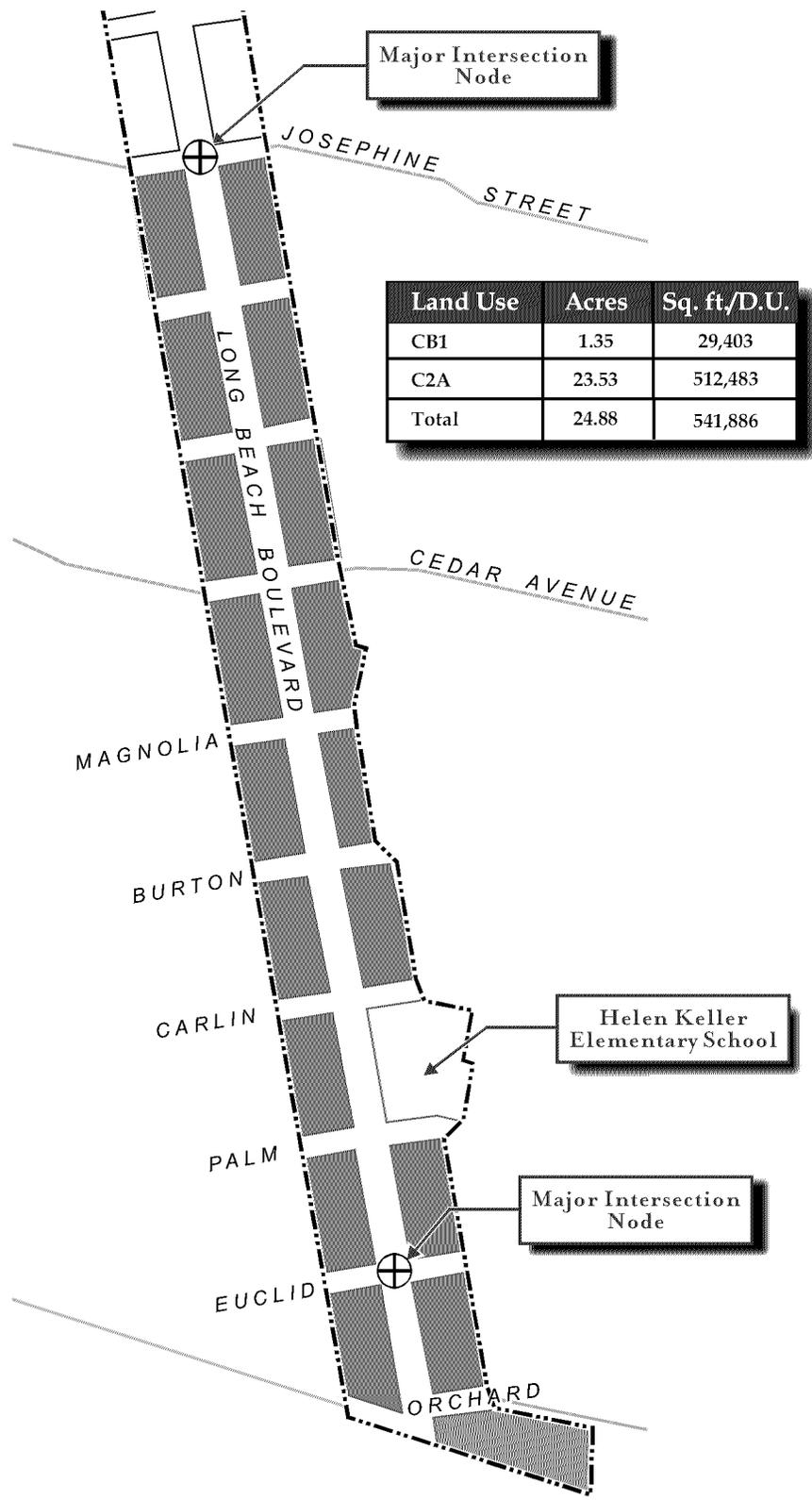
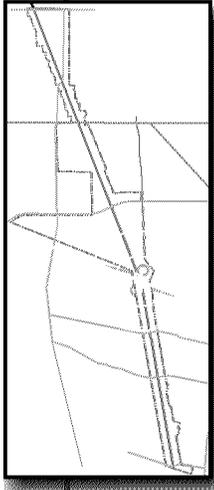
LSA

- REVITALIZATION BOUNDARY
- MAIN STREETS
- MTA GREEN LINE METRO
- TRANSIT VILLAGE GATEWAY
- COMMERCIAL (CB-1, C-2, C-2A, C-3, PCD, HMD)
- MULTI-FAMILY RESIDENTIAL (R-3 & PRD)
- CITY BOUNDARY



Long Beach Boulevard Specific Plan

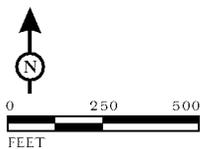
Figure 2.5: Existing Village IV-Business Village Land Use Plan



Land Use	Acres	Sq. ft./D.U.
CB1	1.35	29,403
C2A	23.53	512,483
Total	24.88	541,886

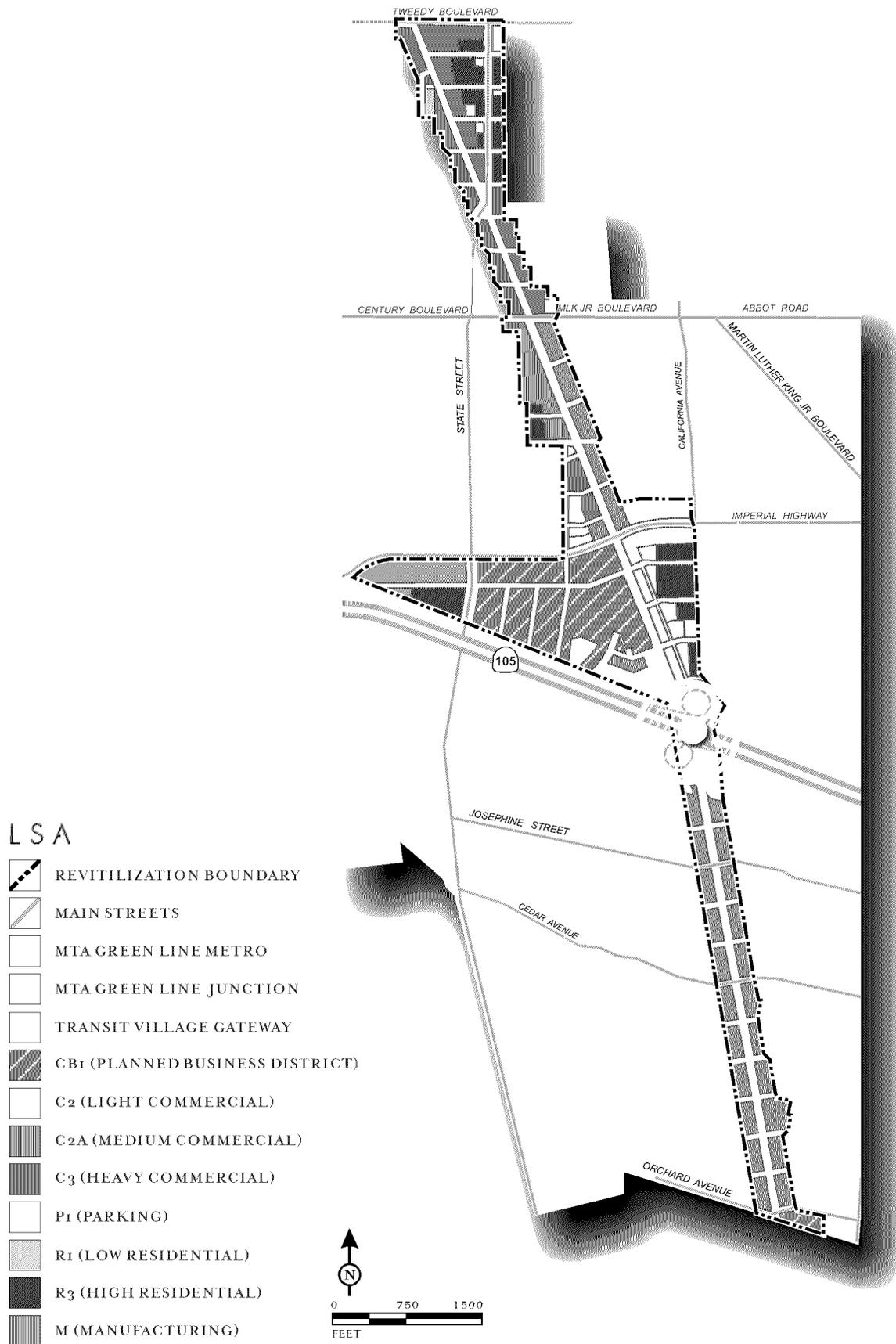
LSA

-  REVITALIZATION BOUNDARY
-  MAIN STREETS
-  COMMERCIAL (CB-1, C-2, C-2A, C-3, PCD, HMD)
-  TOWNHOUSE & CLUSTER (R-2)
-  PUBLIC FACILITY (SCHOOL, CITY, FIRE)
-  CITY BOUNDARY



Long Beach Boulevard Specific Plan

Figure 2.6: Current Zoning Plan



- **Zone C-2:** Light Commercial is located north of I-105 and accounts for 57 parcels. The Light Commercial (C-2) district provides for a limited range of smaller-scale business activities, which serve the needs of residents who live nearby. Typical businesses include, but are not limited to, food and drug stores, childcare, clothing stores, neighborhood-serving convenience stores, and professional and business offices.
 - **Zone C-2A:** One hundred seventy-eight (178) parcels are designated medium commercial zone. This zone accounts for 32 percent of the total project area. The Medium Commercial (C-2A) zone provides for retail centers serving communitywide needs that include, but are not limited to the manufacture of bakery goods; manufacture of ceramic products; manufacture of musical instruments, toys, and novelties; motels and hotels; and places of worship.
 - **Zone C-3:** Heavy Commercial is located along Long Beach Boulevard, between Imperial Highway and Tweedy Boulevard to the north. Ninety-two (92) parcels are located within the heavy commercial zone. It provides retail centers that serve communitywide needs and neighborhood needs. The C-3 zone provides for commercial areas that include, but are not limited to, large retail uses such as big-box stores, furniture stores, and appliance and home electronics retailers; movie theaters; service commercial businesses; professional business offices; and restaurants. It also provides for a wide range of smaller-scale business activities, which serve the needs of residents who live nearby. Typical businesses include but are not limited to, food and drug stores, childcare, clothing stores, and neighborhood-serving convenience stores.
- **Residential Zones**
 - **Zone R-1:** Single-family residential is located within one block (total of six parcels) between Wisconsin and Michigan Avenues. R-1 densities may range up to 7 units per gross acre.
 - **Zone R-3:** Multifamily is located north of I-105, mostly on the west side of Long Beach Boulevard with 115 parcels (20.7% of project area). The High Residential density allows multifamily developments of apartments, condominiums, town homes, stacked flats, single-family attached units, and multistory senior housing. This designation is designed to serve as a transition from single-family to commercial and industrial land uses. Densities may range up to 18 units per gross acre (maximum of 1 unit per 2,420 square feet of gross property area).
 - **Manufacturing Zone**
 - **Zone M:** Manufacturing is located between Imperial Highway and I-105, west of State Street, accounting for 16 parcels or 3 percent of the total number in the planning area. All of the parcels fall under the SPA overlay from the City's General Plan Map.

- **Specific Plans**

- **Zone SPA:** The Specific Plan Area is a land use designation on the City's General Plan Map that is intended to allow for a mix of residential and commercial land uses. Residential density shall be the same as allowed under the R-3 designation, and commercial intensity shall follow what is specified for the Commercial designations. This SPA overlay covers current commercial, manufacturing and multifamily designations.

- **Parking District**

- **Zone P-1:** The Parking district may be combined with all other zones within the City and shall be applied in conformance with a plot or site plan approved as part of any application for discretionary approval or to any municipal or district parking lots established within the City. Uses permitted include off-street parking lots, parking structures, and accessory and sign uses as permitted in the zone with which the P-1 Zone is combined. Outside displays are prohibited.

- **Mixed Uses**

- Residential intensity shall be the same as allowed under the R-3 designations. The commercial intensity shall comply with that specified for the Commercial designations.

The following zoning designations do not currently exist within the project area: Residential Two Family (R-2) and Hospital/Medical/Dental Zone (HMD).

There are some inconsistencies between the City's General Plan and Zoning Plan. There are also some inconsistencies between current property type uses and the current zoning. There are a few parcels within the project area that are either partly in the project boundary or are split between two to three different zoning designations. Property types were taken from the online Los Angeles County Assessor map website. Table 2.C shows how each zone is further organized by property type.

2.3 DEMOGRAPHIC ANALYSIS

2.3.1 POPULATION

The population for the City of Lynwood in 2000 was 69,899 (U.S. Census 2000) and housing units totaled 14,987. Average density for *population* in the City is 13,980 residents per square mile and the average density for *housing units* is 3,090 per square mile.

The Hispanic/Latino population is the largest group at 33,606 (82 %). The white population is 1,116 (3%), the black/African American population is 4,442 (14%), and the other population, including the Asian and Pacific Islander and multi-racial populations, is 768 (1%).

2.3.2 HOUSING TENURE

Renter occupancy is more prevalent than home ownership in the project area. According to Assessor's data, 121 parcels or 22 percent of the total in the planning area are identified as residential, of which 115 are zoned multifamily.

Table 2.C - Zoning and Property Type

Zoning Designation	Total Acres	# of Parcels	Property Type for Parcels					
			SFR	MFR	Com/Indus	Vacant	Other	Blank
R-1: Single Family	0.537	6	6	0	0	0	0	0
R-2: Multifamily	17.565	115	36	65	2	6	5	0
P-1: Parking	4.967	29	8	14	1	3	2	0
CB-1: Planned Business	32.662	70	0	0	20	34	7	9
C-2: Light Commercial	11.249	50	3	2	41	3	3	0
C-2A: Medium Commercial	44.324	178	27	17	103	17	4	9
C-3: Heavy Commercial	20.443	92	1	7	74	7	1	3
M: Manufacturing	6.220	16	1	1	13	1	0	0
TOTAL	137.967	556	82	106	254	71	22	21

This information on the tenure of parcels in the project area corresponds with data available from the U.S. Census. According to Census data, the tracts that fall within the project area contain 8,105 housing units that are occupied by owners or renters (96%), and 337 housing units that are vacant (4%). Of the total number of occupied housing units, approximately 38 percent are owner-occupied. See Table 2.D.

Table 2.D - Housing Tenure

Total Number of Housing Units	Owner-Occupied	Renter-Occupied
8,105	3,075	5,030
100%	38%	62%

2.3.3 ECONOMICS

The local market in Lynwood is generally composed of households with low to moderate income with a significant incidence of Hispanic families. The City's

median household income was \$ 35,888 in 1999 dollars (U.S. Census 2000).

The total workforce living in the City of Lynwood is 20,272 (U.S. Census 2000). The Census tracts surrounding the Project area have a median household income of \$34,110 with about 13,344 people in the labor force. About 40 percent (7,693) of population age 25 and older have less than 9th grade education level, which typically corresponds to low-income employment or unemployment.

2.4 SITE CONSTRAINTS

2.4.1 BLOCK DEPTH

Most of the parcels located within the project area lack adequate depth. Some parcels have irregular and/or undevelopable shapes. A typical parcel along Long Beach Boulevard is 140 feet deep and 50 feet wide; however, the depth of a parcel along Long Beach Boulevard can be as small as 72 feet.



This old building has incoherent exteriors and an unattractive roof fence.

2.4.2 BUILT STOCK

The project area has 10,578 standing structures, of which 2,764 (26%) contain 3 to 9 units. About 66 percent of the structures in the project area were built between 1940 and 1969, and roughly only 3 percent are less than 15 years old. The great number of older structures represents an enormous challenge in updating building façades to create a compatible and fluid street character.



Inadequate pedestrian crosswalks; no striped crosswalk.

2.4.3 SIDEWALKS

There is a significant lack of good sidewalks in the project area. Existing sidewalks are unsafe, deteriorated, and

end abruptly. They lack landscaping and other sidewalk elements.

2.4.4 PARKING

Long Beach Boulevard has a lack of parking along the northern portion of the corridor and a surplus of parking around the freeway.

One of the other issues facing the users of the businesses and services in the Project area is that parking spaces are not necessarily located near the uses.

The City has identified that the current parking spaces are inadequate at most places. This is the case with strip malls located at the major intersections, such as Imperial Highway, and mid-block businesses along Long Beach Boulevard.

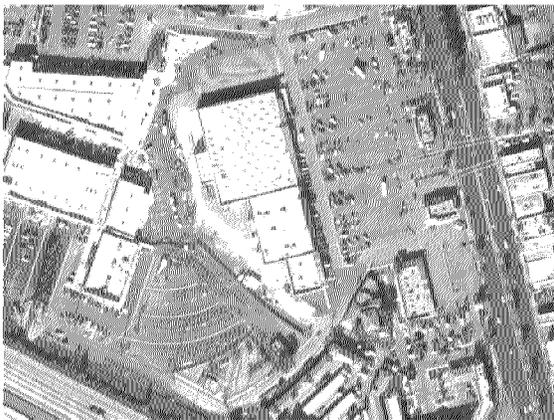
Paradoxically, parking under the part cloverleaf and under I-105 is only marginally utilized at most times. There is huge number of parking spaces available at the transit station as seen in the aerial photograph below.



Underutilized parking under I-105.

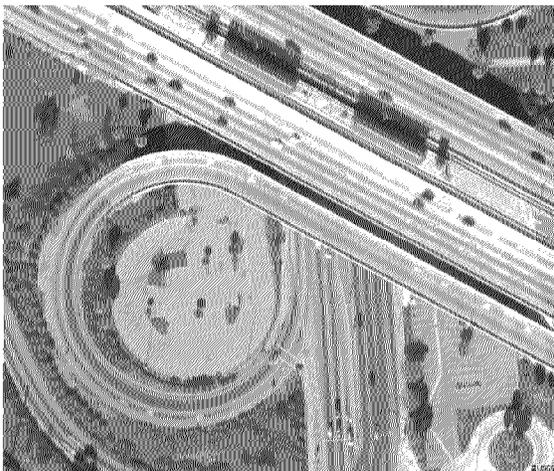
Underutilized parking exists, particularly in the designated parking lots under I-105 for the MTA ridership, which are

currently untenanted resulting in concern for public safety and constraints within the Specific Plan development. Another underutilized parking lot is just north of Pluma Street on the west side of Long Beach Boulevard. The parking lot serves a relatively large shopping center, but has many vacant spots. An aerial view of a parking lot north of I-105 and west of Court Street shows it to be completely empty as seen in the picture below.



Example of a vacant parking lot.

It is envisaged that the implementation of this Specific Plan will result in frequent use of the parking lot under I-105 and other lots due to the increase in development.



This aerial photograph of the parking lot below the freeway illustrates the typical underutilized conditions near the intersection.

2.4.5 SOCIAL PROBLEMS

Graffiti and litter are found throughout the project area. This seems to relate to the location of liquor stores, pawn shops, low-budget motels, and cash checking stores. There is a moderate to high amount of prostitution, theft, disorderly conduct, vagrancy, non-aggravated assaults, and drug activity within the City and the project area.



Graffiti on a storefront.

2.4.6 TRANSIT ACCESS

Access to the transit stop is not at grade and has poor design-lighting and visibility. There are psychological and real barriers to "walkability" as Long Beach Boulevard is a wide, busy road with an incomplete sidewalk network.



Pedestrian crossing and off-ramp.



Transit station and the freeway overpass view from Long Beach Boulevard.



Parking and access to the Green Line located under I-105.

Transit Station Crime and Safety. A 1997 study¹ conducted by UCLA professor, Anastasia Loukaitou Sideris, et al. looked at the incidence of crime on the Green Line and some of the factors related to the physical layout and access to the stations. Along the Green Line, the Long Beach Transit Station has the second-highest incidences of crimes including petty theft, disorderly conduct, vagrancy, non-aggravated assaults, drug violations, but not serious crimes such as murder. The report also showed the locations of crimes committed within the stations. For Long Beach (Lynwood), 30 crimes were committed at the elevator/stairs, 19 on the platform, and 10 occurred in the parking lot. According to this study, the main influencing factors for high crime were the following:

- Minimal visibility from their surroundings.
- Long walking time from the parking lot to the platform.

¹ "The Geography of Transit Crime: Documentation and Evaluation of Crime Incidence on and around the Green Line Stations in Los Angeles."

- Large parking lots with no pedestrian activity.
- Dark, desolate parking areas under the freeway.
- Poor maintenance and abundance of graffiti and litter.
- Close proximity to certain land uses (bars, liquor stores, taverns, check cashing stores).
- Having only residential uses in the station neighborhood.
- Neighborhoods with littered sidewalks and deteriorating building stock.
- High population density in the station neighborhood (9,352 population).
- Overcrowding (average of 4.6 persons per household).
- Low-income neighborhoods (39% with income less than \$25,000).
- Higher concentration of youths (49% of population 24 years and under).
- Higher percentage of population with less than high school education (20%).

The report also identified conditions to help mitigate the area's crime rate:

- Appropriate station and parking lot design with no entrapment spots or hiding places.
- Smaller, well-lit lots that integrate well to the surrounding area.
- Open convenience stores and ticket machines in the parking lot.
- Use of graffiti and vandal-resistant materials.
- Good maintenance, cleanliness, well-kept area.
- Mixed use with office or light industrial uses.
- Surveillance and security guards/police.
- Security Audit by transit authorities.
- Adequate lighting.
- Good building stock.

2.5 TRANSPORTATION

The existing transportation system in the City of Lynwood includes regional and local connections.

2.5.1 THE CIRCULATION NETWORK

Regional and national access to the City of Lynwood is provided by I-105 and the Long Beach Freeway (I-710). The major arterial street layout in Lynwood supports the regional access from the adjacent freeways. Similar to a typical grid pattern, arterial streets run north/south and east/west. The arterials are typically paved to 50 feet within an 80-foot right-of-way but may expand paved to 70 feet within a 100-foot right-of-way.

The Specific Plan Area lies along Long Beach Boulevard, which runs north and south through the center portion of the City. It is one of the key arterials in the City and provides six to four lanes of travel. Additional key arterials include Martin Luther King Jr. Boulevard, Imperial Highway, Alameda Street, and Atlantic Avenue.

2.5.2 THE TRANSIT NETWORK

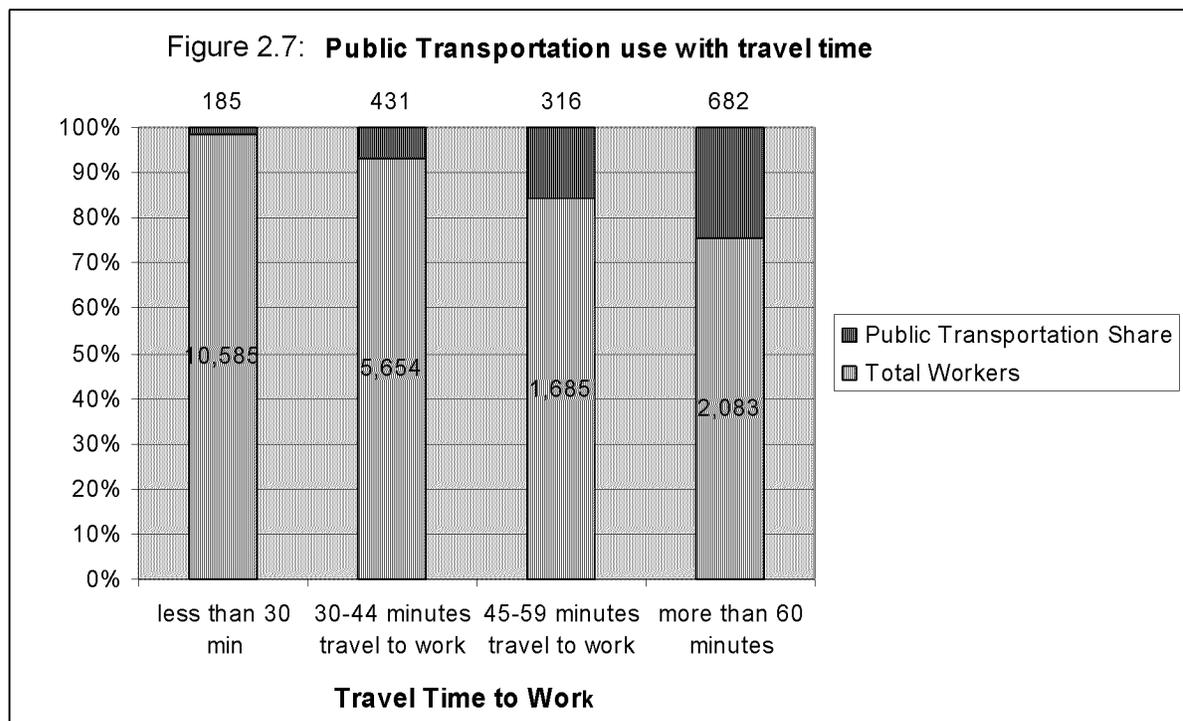
The City of Lynwood has three major components of transit service: The Metro Green Line, Metro Bus Lines, and local trolley and Dial-a-Ride.

The Metro Green Line is a light rail that starts west of Studebaker Road in Norwalk and covers about 17 miles along the median of I-105. It has 14 existing stations and serves Los Angeles, Los Angeles International Airport (LAX), Hawthorne, El Segundo, Norwalk, Downey, Lynwood, Redondo Beach, Inglewood, and connects to the Metro Blue Line (which travels from Long Beach to Downtown Los Angeles).

The Metro Buses in the City of Lynwood service around 26 communities in southeast Los Angeles County. The following routes intercept the City of Lynwood and provide regional service for residents and visitors of the community.

- **Line 60:** Travels from Downtown Los Angeles to Downtown Long Beach and operates 24 hours per day.
- **Line 251:** Avenue 26 and North Figueroa Street, connects to Cypress Park and Lincoln Heights.
- **Line 622:** Aviation Green Line Station. Service in early a.m. only.

Figure 2.7: Public Transportation use with travel time



- **Line 360:** Connects to downtown Los Angeles Monday through Friday. Service during rush hour is limited.
- **Line 751:** Connects the City of Lynwood to Lincoln Heights.
- **Line 252:** Provides connection to Boyle Heights and the City of Lynwood; stop is located at Soto and Marengo.

Local service in the City of Lynwood includes a Dial-a-Ride system and the Lynwood Trolley Company. The trolley includes four routes that service residential and local business areas:

- Route A referred to as the central loop;
- Route B covers the eastern area of the City;
- Route C services the outer loop; and
- Route D includes the Imperial Highway segment.

2.5.3 CURRENT MODES OF TRANSPORTATION

Travel time to work and the use of public transportation is best illustrated in Figure 2.7 based on data from U.S. Census 2000. As the travel time increases (distance from Lynwood), the reliance on public transportation is higher, with the caveat that it takes more travel time to use transit if one is working near downtown Los Angeles. A total of 1,614 (8%) of the 20,007 workers (who work outside the home) use some mode of public transportation.

In the Census tracts surrounding the project site, 9 percent of the residents use public transportation, as seen in Table 2.E. Of the total public transit users, 4 percent use the rail system and 94 percent use the bus or trolley bus.

Table 2.E - Means of Transportation To Work

Mode of Transportation	Amount	Percentage
Vehicle	9,386	85%
Motorcycle	14	< 1%
Public Transportation	950	9%
Bicycle	64	1%
Walk	343	3%
Other Means	118	1%
Work at Home	157	1%
TOTAL	11,032	100%

2.5.4 STREET CHARACTER

Long Beach Boulevard is primarily a north-south orientation. Traffic is heavy and pedestrian safety is limited. The Boulevard can be described as inconsistent in its physical appearance.



An example of newer development along Long Beach Boulevard.

Several areas combine newer developments with older segments and the difference is striking.

Existing pedestrian crossings are sparse, making it difficult for individuals to cross Long Beach Boulevard due to the distance between crossings and existing light protection.



An example of an older segment of the Boulevard.

Mature street trees exist along sections of the Long Beach Boulevard project site. In addition, there are medians with various and/or little vegetation. Medians within the project site exist primarily around areas closest to I-105 off-ramps.

2.6 PUBLIC INFRA-STRUCTURE

2.6.1 STREETS, LIGHTING, AND PUBLIC LANDSCAPE

Long Beach Boulevard lacks consistent median design, public landscape, and lighting along the corridor. The island median seen in the following picture occurs between I-105 and Martin Luther King Jr. Boulevard. There are no island medians between Beachwood and Oakwood Avenue.



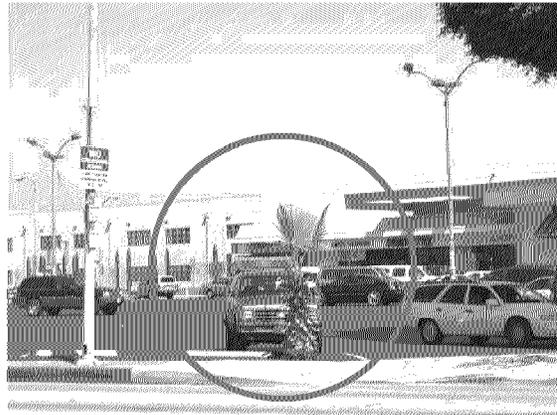
Existing median conditions, just north of I-105 approaching Martin Luther King Jr. Boulevard.



Existing streetscape with uniform lighting that is auto-oriented and not pedestrian-friendly.



Located along the northern portion of Long Beach Boulevard, this block shows no landscaping.



There is no landscape buffer between the parking lot and the sidewalk. The small palm tree seems misplaced and awkward.

South of I-105, the island median is consistent from the off-ramp to Burton Avenue.

Streetlights along Long Beach Boulevard are consistent and spaced well; however, they are not scaled to pedestrians or the numerous one-story buildings.

Landscape along most of the Boulevard includes various plant palettes, but there are no consistent themes along the sidewalks. There are good examples that exist where landscape is used to separate properties, offer a buffer between parking lots and the sidewalk, or as corner

treatments. A few areas have either used landscape in a detracting manner or lack landscaping.

2.6.2 GAS AND ELECTRICITY

Electricity is provided to the project area by Southern California Edison. The electric supply is sufficient to service the future needs of the City.

The Southern California Gas Company supplies natural gas to the project area through a fixed transmission and distribution area. Several major natural gas mains pass through Lynwood.

Available natural gas supplies are sufficient to meet the existing needs of the proposed project.

2.6.3 STORM DRAINAGE AND SEWER

Sewage disposal service is provided to the project area by the City of Lynwood Public Works Department. City lines carry sewage to Los Angeles County trunk lines, which transport sewage to the Joint Water Pollution Control Plant in the City of Carson. Most of the properties in the City are served by sewer. The only area on septic tanks is the industrial area located north of I-105 and west of Long Beach Boulevard.

The City's storm drain system consists of five major north to south drainage facilities. These include the following:

- The State Street system serves the drainage area generally west of Long Beach Boulevard.
- The Bullis Street system drains the area generally east of Long Beach Boulevard and several blocks east of Bullis Road.
- The eastern system consists of three drains that cut from the northwest to the southeast and enter the Los Angeles River.

2.6.4 WATER SUPPLY

The water supply to the project area is provided by the City of Lynwood Public Services Department and Park Water Co. The primary source of water comes from groundwater aquifers. The City owns and operates six active wells and one 3 million-gallon reservoir. In addition, there is a 16-inch Metropolitan Water District (MWD) feeder to the reservoir. According

to the City's General Plan, the City pumps about 5,500 acre-feet per year from the MWD feeder line. When the reservoir falls to four feet, the MWD feeder automatically activates. Water lines are located throughout the City ranging from 6 to 16 inches in diameter. As of 2002, the City was found to have had an adequate water supply to serve its population.

2.6.5 CABLE, INTERNET, AND TELEPHONE

Telephone service to the project area is provided by SBC, now joined with AT&T. SBC|AT&T coordinates the installation of its facilities with other utility companies in order to run its cables alongside other company lines in the same trench or on the same pole. Unless a plant facility is needed, SBC|AT&T will provide the funds to supply telephone service to the site directly, or a reimbursement agreement with the developer will be prepared. The City requires that all new improvements be placed underground. SBC|AT&T also has the ability to provide high-speed Internet access for computers using lines to a fiber-optic network.

Long-distance telephone service to the project area is offered by SBC|AT&T, as well as MCI, U.S. Sprint and other competing companies.

AT&T Broadband, AT&T|DISH Network, and AT&T Yahoo! provide television service to the project area by cable and satellite. AT&T has lines extended throughout the area, and service extensions are provided as development occurs.

2.7 MARKET CONDITIONS

2.7.1 EMPLOYMENT AND POPULATION GROWTH

Employment growth in Lynwood is forecast to exceed household growth by a slim margin, maintaining a healthy jobs-housing balance. Over the next 20 years, the City's population will increase by approximately 6,477 new residents (9%), while the employment growth increases by 2,656 new jobs (18%). The growth rates for the City are lower than Los Angeles County as a whole, suggesting a slow-growth development for housing; however, employment growth shows the City is only 4.4 percent behind the County. These figures suggest that the City needs a wide range of housing types to attract the local workers to find

residence within the city limits. See Table 2.F.

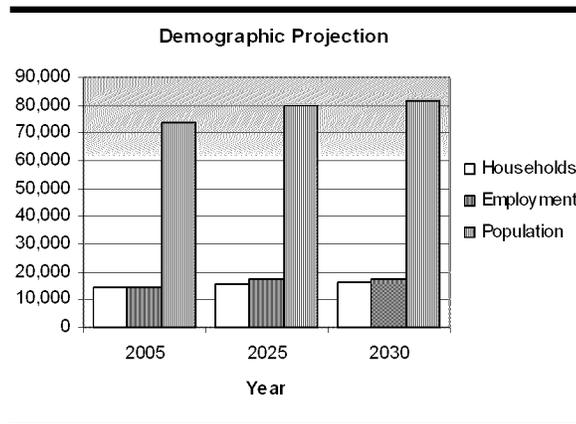
2.7.2 REAL ESTATE

There is a strong need for redevelopment in Lynwood due to numerous old buildings, run-down shopping centers, and discontinuous and disjointed development. Existing and proposed retail shopping centers are likely to meet current demand as well as additional demand in the foreseeable future term. Overall, the area is not competitively located with regard to the development of a regional serving retail center. Future commercial retail and mixed-use developments will likely serve the local residents, nearby employees, and commuters.

Table 2.F - Demographic Projections

Year	Population	Households	Jobs
2000	70,161	14,406	14,416
2005	73,544	14,517	14,561
2010	75,067	14,688	16,052
2015	76,755	15,085	16,467
2020	78,424	15,489	16,863
2025	80,021	15,893	17,217
2030	81,551	16,298	17,535

Source: 2004 RTP Growth Forecast-SCAG



3.0 LAND USE PLAN

3.1 INTRODUCTION

Long Beach Boulevard is a regional corridor starting in Los Angeles, passing through South Gate, Lynwood, Compton, Paramount, Carson and ending in Long Beach. The future of Long Beach Boulevard in Lynwood is envisioned as a viable mixed use, medium-intensity corridor within the region as well as a downtown main street for the City.

The focus of the land use plan is to make efficient use of the land and promote and utilize the MTA Green Line Metro station, while creating an aesthetically pleasing built form and improving the safety and vitality of Lynwood. The land use plan provides for the retention, protection, and commercial use of existing structures with a variety of community-serving uses within the Long Beach Boulevard area.

The proposed land uses, development standards, and design guidelines are intended to be tools for transition of the Boulevard from a well used but physically run-down strip to a vibrant, community-serving commercial center surrounded by a mix of uses. These employment-generating uses have been designed to capitalize on the site's proximity to the MTA Green Line Metro station.

Long Beach Boulevard provides the opportunity for rapid public commuter transportation to accommodate residents of all income levels, and a mix of land uses and intensity supported by a thriving, dense community in the region.

3.2 PROJECT GOALS & OBJECTIVES

The primary goal of the project is to create a revitalized medium-intensity corridor that functions as a downtown for the City.

The following objectives underlie the land use concept plan:

- To capture regional traffic along Long Beach Boulevard and link with surrounding neighborhoods by creating an attractive mixed-use development that is as a destination as well as a neighborhood-serving complex.
- To apply principles of transit-oriented development to the central portion of project area.
- To develop the Boulevard with a streetscape design theme that creates a sense of place and positive identity.
- To provide attractive public spaces to serve visitors and residents.
- To develop a street design with pedestrian paths and bike paths that contributes to a system of fully connected network.
- To encourage the presence of people in public spaces at most times for safety and vitality.
- To instill the "town center" as a venue for special community events as a means of generating continued interest in the businesses and housing opportunities located here.
- To provide a framework for approval of incremental development projects on a single concept plan that offers

defined ranges of flexibility to accommodate market changes.

- To let the market shape conducive development with varying mixed-uses.
- To accomplish the balance between development guidance and flexibility responding to the opportunities related to this site through the adoption of a specific plan.

3.3 PROJECT POLICIES

The Long Beach Boulevard Specific Plan establishes a land use plan and framework of standards and guidelines for the purpose of revitalization of the corridor. This Specific Plan approaches the Boulevard as an entity that is part of a larger corridor as well as its desired function as a downtown for the City of Lynwood.

Five major policies that are the guidance to revitalize the corridor are:

1. Establishment of four villages with distinct functions and focus;
2. Transit-oriented development around Metro Green Line
3. Combination of land uses, particularly mixed uses;
4. Strong architectural and landscape character; and
5. Pedestrian connectivity to transit.

3.3.1 ESTABLISHMENT OF VILLAGES

The proposed land use plan arranges the 2.3-mile long Boulevard into four villages based on the existing land uses, major transportation corridors, and TOD radius size. Figure 3.1, Conceptual Village

Location Plan illustrates the four distinct villages and their relationship to I-105.

The proposed Downtown Village I, covering 27 acres, is aimed at supporting the numerous existing small businesses between Tweedy Boulevard and Martin Luther King Jr. Boulevard. No changes in the land uses are proposed in this village; however, the architectural and landscape design guidelines will guide the developments in this village. The existing small businesses are part of the local economy and cater to regular/daily retail needs of the community. At all times, it is a vibrant area with heavy movement of people and vehicles. It is clearly an important part of the City's small business economy.

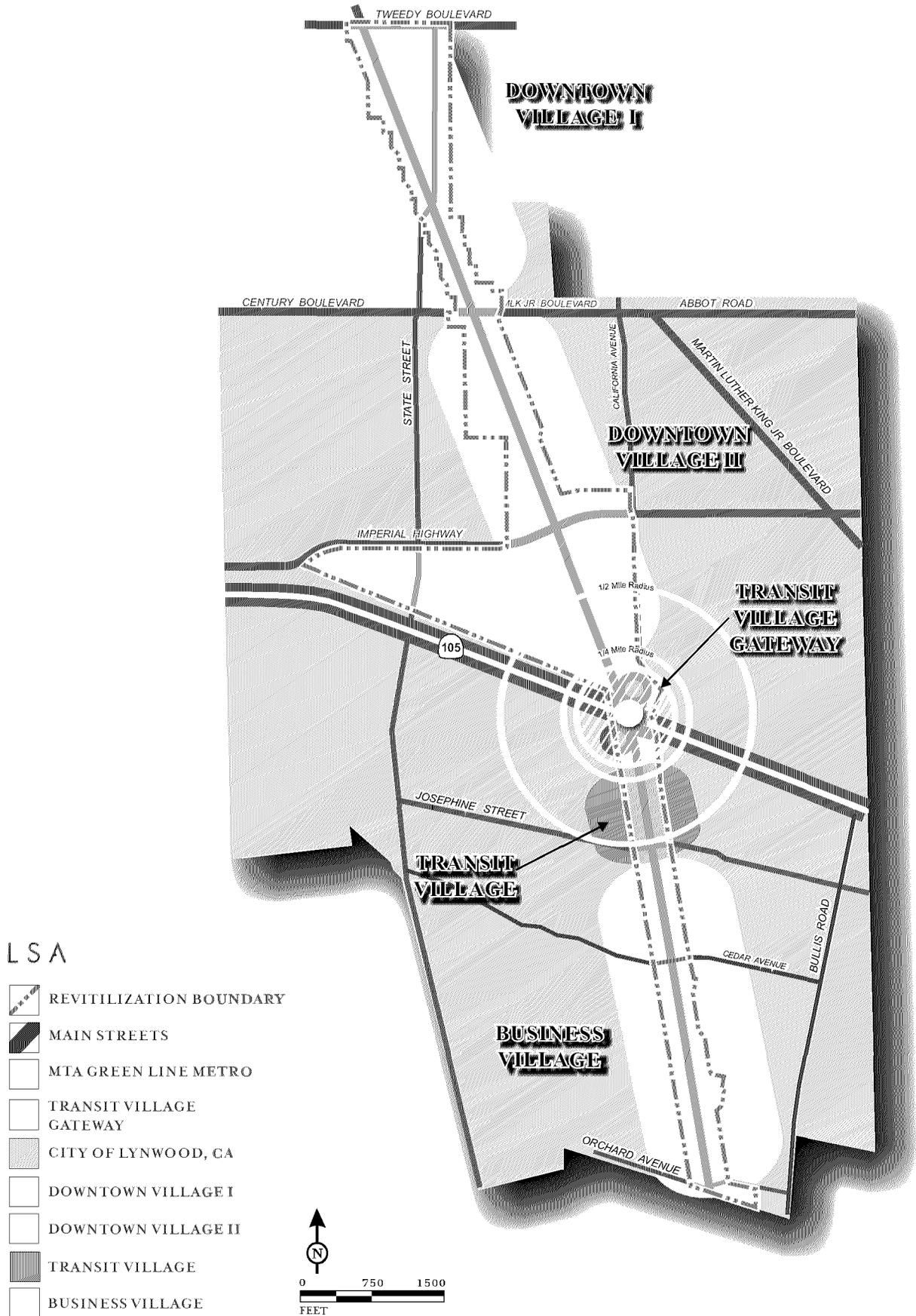


Plaza Mexico has many pedestrian-friendly elements such as scaled-down lighting, landscape, fountains, and proper signage.

Proposed Downtown Village II-Mixed-Use Village is a key area between Martin Luther King Jr. Boulevard and the interchange of I-105 and Long Beach Boulevard. This 81-acre village underwent major change with the addition of Plaza Mexico, a 36-acre, retail development set within open air plazas with Mexican themes and catering to the demand in the region. The proposed mixed-use district over the Specific Plan Area (see the

Long Beach Boulevard Specific Plan

Figure 3.1: Conceptual Village Location Plan



General Plan map) establishes the mixed-use focus for the City with emphasis on walkability and a combination of retail, office, civic, and residential uses set amidst a pedestrian-oriented layout. The development standards for this village provide for the creation of an internal "Main Street." The development envisaged in this village provides for ground-level pedestrian-oriented businesses and second level/third level offices or residential units. This would create a highly livable district with convenience of the transit nearby. The synergy created by the land use mix within Village II is aimed to reinforce the use of the MTA Metro Green Line on I-105 and Long Beach Boulevard.

Village III-Transit Village at the south of the intersection of I-105 and Long Beach Boulevard is set within the 0.5-mile radius of the Metro Green Line station and comprises approximately 5 acres. The land uses envisioned in this village are supportive of the transit station. They include childcare facilities, a library, and clinics among other uses that have a natural synergy with the transit needs. The Specific Plan proposes that transit-serving and neighborhood-serving commercial uses be a priority in this village.

The proposed architectural and landscape design guidelines for Downtown Village II-Mixed-Use Village and Village III-Transit Village are consistent. Visually, these two villages will establish continuity along the intersection of I-105 with Long Beach Boulevard. TOD criteria guide the establishment of standards in Village II and Village III.

Village IV-Business Village, at the southernmost end of the Specific Plan area, covers approximately 25 acres. The existing medium commercial zone north of

Burton Avenue and south of Josephine Street is proposed as a mixed-use land use with retail and commercial uses.

Live/work mixed use developments are envisioned south of Burton Avenue and north of Orchard Avenue. The introduction of these developments will bring business and professionals who need work space and housing in close proximity to Long Beach Boulevard, I-105, and the Metro Green Line station. This district implements live/work development guidelines for this village.

The architectural and landscape design guidelines are intended to propel a conscious development for attracting small professional offices that would revitalize this part of Long Beach Boulevard.

3.3.2 TRANSIT-ORIENTED DEVELOPMENT (TOD)

California State Department of Transportation defines TOD as a moderate to higher density development, located within an easy walk of any major transit stop, generally with a mix of residential, employment, and shopping opportunities designed for pedestrians without excluding the automobile. TOD can be new construction or redevelopment of one of more buildings whose design and orientation facilitate transit use.

As described in Chapter 1.0, the salient features of transit oriented development are mixed-use projects with higher intensity and pedestrian and bicycle connectivity. Such development encourages an appropriate mixture and density of activity around the transportation center, and promotes alternative modes of transportation. It

decreases automobile-dependency and mitigates the effects of congestion and pollution.

Transit-oriented development assists in achieving the following:

- Encourages people to walk, ride a bicycle, or use transit;
- Allows for a mix of uses to create an environment that engages people at the pedestrian scale;
- Achieves a compact pattern of development that is more conducive to walking and bicycling;
- Provides a high level of amenities such as attractive landscaping, benches, bicycle racks, and public art elements that create a comfortable environment for pedestrians, bicyclists, and other users;
- Maintains an adequate level of parking and access for automobiles;
- Creates fine-grained detail in architectural and urban form that provides interest and complexity at the level of the pedestrian and bicyclist;
- Encourages uses that allow safe round-the-clock activity around transit stations;
- Provides sufficient density of employees, residents, and recreational users to support transit; and
- Generates a relatively high percentage of trip riders serviceable by transit.

The proposed Downtown Village II and Transit Village around Metro Green Line demonstrate these policies for land uses and architectural and landscape guidelines.

3.3.3 COMBINATION OF LAND USES

This is fundamental to the successful development of Long Beach Boulevard as a medium-intensity corridor. Four proposed land use categories are commercial (15.5%), residential (4.9%), public facility (3.1%), and mixed use (76.5%).

Mixed use is the main land use category. In response to the different villages and their focus for future development, mixed use is further defined into two categories. These are:

- Retail/Commercial/Residential; and
- Live/Work.

To better understand the concept, the following section explains the characteristics of these uses, their application, examples, and outcomes related to the mixed use.

1. Retail/Commercial/Residential Mixed Use.

Retail/Commercial/Residential mixed use can be utilized for a single building or an entire development over many parcels. Buildings such as older warehouses, big-box stores, industrial buildings, or any underutilized, run-down buildings are key structures to convert to this type of mixed use. Typical mixed-use projects include having a mix of specialty retail, restaurants, cafés, offices, and residential uses. Current examples of mixed-use projects can be found in Long Beach, Brea, Fullerton, Anaheim, Santa Ana, Pasadena, Davis, Emeryville, and Huntington Beach, California. There are over 302 cities/counties in California that have zoning ordinances allowing mixed use.

An important element for designing mixed-use buildings is the public space for

interaction. To foster a safe community setting, the design of the public space along the building entries can be the difference between an uninvolved structure and a fully functioning place where people cross paths and socialize. The layout should shape open spaces such as an atrium, plaza, or courtyard between the buildings along an entry path that encourages casual interaction; places where it is comfortable to have a meal or socialize. Long Beach Boulevard workers and residents would be able to live, work, and shop in the same neighborhood, thereby reducing commute time and pollution, and contributing to community safety as a result of moderately intense uses.

Within mixed use projects, the retail/commercial and residential proportion varies for TOD projects across California. The retail/commercial component is as low as 5 percent in some projects (e.g., Villages of La Mesa-Amaya Trolley Station at La Mesa) to 88 percent (e.g., Gateway Plaza-Union Metro Station at Los Angeles). The residential component varies from 7 percent (e.g., Gateway Plaza-Union Metro Station at Los Angeles) to 91 percent (e.g., the Crossings-San Antonio Caltrans Station at Mountain View).

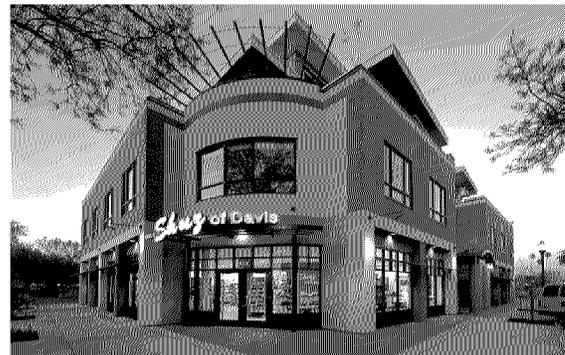
The retail/commercial/ residential mixed-use areas are planned in Downtown Village II for the blocks between Martin Luther King Jr. Boulevard and I-105. Based on the existing land, it is estimated that there may be up to 81 acres available for this type of mixed use in the Downtown Village II, or roughly 59 percent of the entire Specific Plan area.

The retail/commercial/residential mixed-use areas are also planned in the Transit Village for the blocks between I-105 and

Josephine Street, and the upper portion of the Business Village, which will include blocks down to Burton Avenue. Based on the existing land, it is estimated that there may be up to 16 acres available for this type of mixed use in the Transit Village and part of the Business Village or roughly 12 percent of the entire Specific Plan area.

The mixed-use buildings require enhanced attention to architectural quality because of the balance between professional office, retail shopping and housing. The architectural design guidelines explained in Chapter 4.0 provide details on balancing public space necessary for retail with office design.

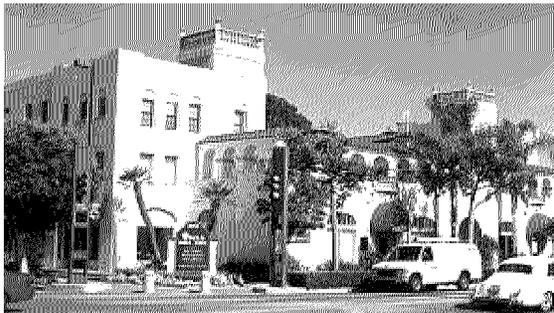
Current Mixed Use Examples:



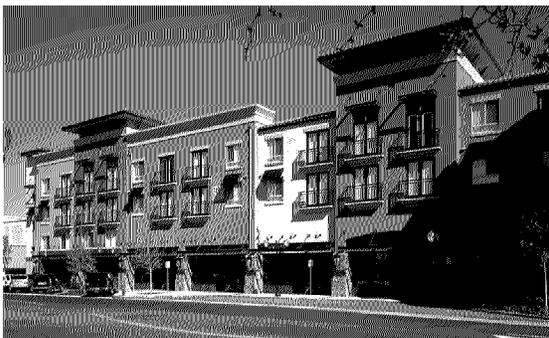
The Chen Building, Davis, California. 2005 Gold Nugget Award Winner for Best Mixed Use Project. Builder: Walsh & Forester, Inc. Developer: Aggie Enterprises, Inc. Architect: McCandless & Associates Architects, Inc. Source: www.goldnuggetawards.com



A mixed-use building in Santa Ana has a restaurant on the first floor with apartments above. Source: "A Tool Kit for Mixed-Use and In-fill Development for Western Riverside County and Orange County," 2004.



Downtown Fullerton has revitalized historical buildings with a mix of uses next to its transportation center. The City has also utilized surface parking lots in key downtown locations to develop mixed-use luxury apartments and cafes. Source: www.ci.fullerton.ca.us.



The Prado, Valencia, California. 2004 Gold Nugget Awards Grand Award for Best Mixed Use Project—Builder/Developer: The Hanover Company. Architect: Thomas P. Cox Architects, Inc. Source: www.goldnuggetawards.com.

2. Live/Work Units. Live/work units are part of a small-scale mixed-use alternative. In the past, they have been used for small artist and craftsman operations, where the resident has a workshop or studio on the premises. Buildings such as older warehouses, industrial buildings or any underutilized buildings are key structures to convert to a live/work product. Live/work structures can also be designed for middle class professionals. Current examples of live/work units can be found in Long Beach, Pomona, Ontario, Corona, Santa Ana, Newport Beach, Ladera Ranch, and Oakland, California. There are about 84 cities/counties in California that have zoning ordinances allowing live/work (sometimes called artists' lofts) arrangements.



Sample courtyard for live/work units in Oakland. Source: www.live-work.com.

Live/work units can either have a predominant use of residential or commercial activity. If the unit is predominantly residential, then employees and walk-in trade are not usually permitted. The units that are more commercial-oriented may have more noise, odors, or other impacts, as well as employees, walk-in trade, or sales. Designing the common spaces with the unit entries opening onto each other will further increase the opportunities for casual interactions.

Proprietors would be able to live above their shops or offices, thereby reducing commute time and pollution, and adding security round the clock for the local neighborhood.

The live/work units are planned in the Business Village for the blocks between Burton Avenue and Orchard Avenue along Long Beach Boulevard, not including the recently constructed Helen Keller Elementary school. Based on the existing block sizes, it is estimated that there may be up to 13 live/work units built per block giving a total of 91 live/work units for the Business Village.

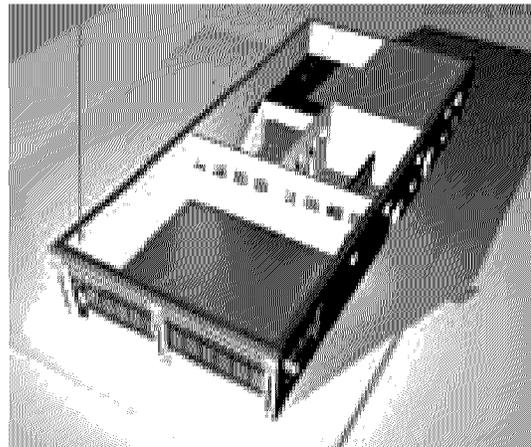
Live/work units also require enhanced attention to architectural quality because of the delicate balance between life and work. The architecture design guidelines are explained in more detail in Chapter 4.0.

Live/work units are finely detailed inside to differentiate the living space and provide parking for unit owners and guests.

Current Live/Work Examples:



East Village Lofts, Santa Ana, California. Santa Ana Artists Village, an eight-block neighborhood of former urban blight has been transformed into a vibrant downtown community with 40 live/work units using a three-story urban loft style, sold at market prices in 2003. Source: www.goldnuggetawards.com.



Interplayce, Oakland, California. The vacant boarded-up 1920s retail building at the ground level has been renovated into a 1,400-square foot dance studio with apartments in the back. New windows glazed with art glass allow daylight into the studio while preserving the dancers' privacy. Source: www.live-work.com.

3.3.4 ARCHITECTURAL DESIGN AND LANDSCAPE GUIDELINES

The architectural character is defined by proposed land use and development standards and its resulting scale and form of structures. Two major architectural styles recommended are Spanish Colonial and Italianate. They are further explained in Chapter 4.0.

Long Beach Boulevard is strongly identified by a unified streetscape design with uniform sidewalk and planting palette. In order to further define villages, major intersections have additional landscape features including sign monuments. Landscape details are provided in Chapter 5.0.

3.3.5 PEDESTRIAN CONNECTIVITY AND SAFETY

The sidewalks and crosswalks along Long Beach Boulevard will be improved to provide for a safer and more comfortable pedestrian environment and to establish the conditions conducive to pedestrian-oriented development. Specific intersections have been identified as needing additional crosswalks as illustrated in Figure 3.2 Pedestrian Pathways and Crosswalks.

A system of enhanced pedestrian pathways is proposed through the blocks on both sides of Long Beach Boulevard connecting from Martin Luther King Jr. Boulevard to Josephine Street, which, over time, would be improved and developed with more pedestrian-oriented uses. Developments should include the pedestrian pathways with courtyards and/or plazas as identified in Figure 3.2 as conceptual locations for pedestrian paths.

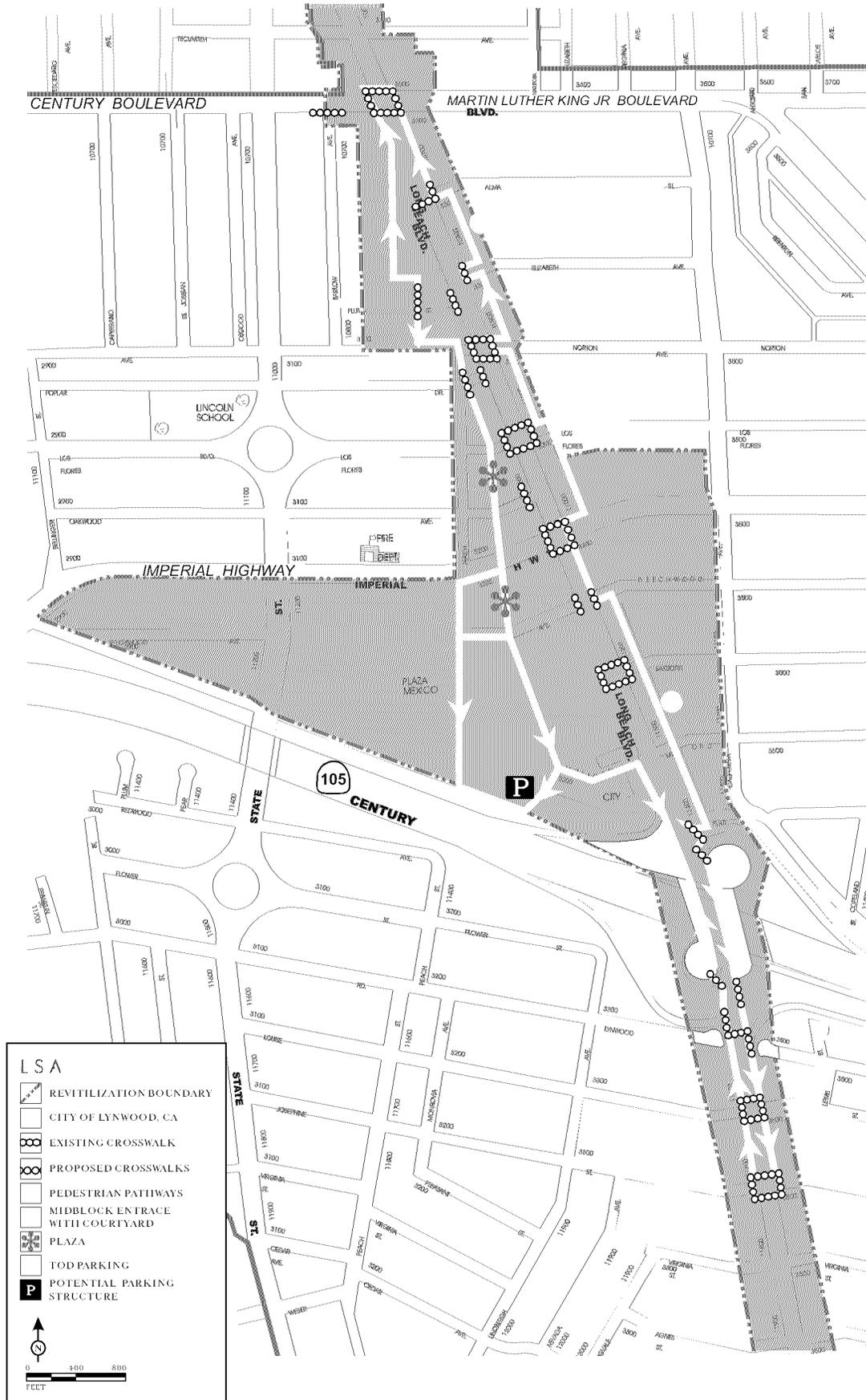
Long Beach Boulevard and its busy intersections should be designed with traffic-calming elements such as traffic-calming circles in the intersections and pedestrian bulb outs at the corners. Chapter 5.0 further defines street elements that ensure pedestrian visibility and safety.

Table 3.A - Proposed Land Use Summary

Proposed Land Use Category	Acres	No. of Parcels	FAR	Square Feet	Maximum DU/AC	Maximum DUs
Commercial	20.22	89	0.5	440,391		
Public (School)	4.29	20				
Sub-Total Commercial/School	24.51	109		440,391		
Single-Family	.54	6			7	4
Multifamily	6.24	47			18	112
Sub-Total Residential	6.78	53				116
Mixed Uses				@ 75% acres for retail/commercial		@ 25% acres for residential
Retail / Commercial / Residential [Village II]	81.40	270	0.7	1,861,537	30	610
Retail / Commercial/Residential [Village III & Village IV]	17.33	72	0.7		30	130
Live / Work [Village IV]	7.93	43				91
Sub-Total Mixed Use	106.66	385		2,257,857		831
TOTAL	137.95	547		2,698,248		947

Long Beach Boulevard Specific Plan

Figure 3.2: Pedestrian Pathways and Crosswalks



3.4 PROJECT DESCRIPTION

3.4.1 PROPOSED LAND USE

Two key elements defining the Long Beach Boulevard proposed Land Use Plan are:

1. The combination of land uses, particularly mixed uses; and
2. Strong pedestrian connectivity.

The total project area is 137.6 acres and incorporates about 547 parcels as seen in Table 3.A-Proposed Land Use Summary.

As enumerated in Table 3.A, Proposed Land Use Summary, the land uses are not changed in proposed Downtown Village I.

The proposed land use adheres to the existing land uses, density, and floor area ratio in 22.6 percent (31.3 acres) of the project site. The commercial uses in this portion of the project will generate 440,391 square feet of built space and 116 residential units.

Table 3.A-Proposed Land Use Summary enumerates mixed uses in the villages in the Specific Plan. Mixed uses are proposed for 77.4 percent (106.6 acres) of the project site. The proposed maximum floor area ratio is 0.7 and maximum allowable density for residential ranges is 30 dwelling units per acre.

For the purpose of summary, it is assumed that retail/commercial component will be built for 75 percent of the site and residential for the remaining 25 percent. Thus, 2.25 million square feet of retail/commercial and 831 dwelling units will be generated from all the mixed uses.

Overall in the total project area, there will be 947 dwelling units and 2.7 million

square feet of retail/commercial built space along Long Beach Boulevard.

The four main types of land uses proposed in the Long Beach Boulevard Specific Plan (not including parking) are Commercial, Residential, Public Facility, and Mixed Uses.

Commercial. Commercial (non-mixed use) includes CB-1: Controlled Business, C-2: Light Commercial, and C-3: Heavy Commercial. Commercial land use is found in Downtown Village I and one block in Village IV-Business Village as detailed later in this section. Commercial land uses comprise 14.6 percent of the total project area.

Residential. Residential (non-mixed use) includes R-1: Single-Family Residential and R-3: Multifamily Residential located in Downtown Village I only. Residential land uses make up 4.9 percent of the total project area.

Public Facility. School is the only land use in this category within the Specific Plan and is located in Village IV-Business Village. This elementary school is built on 4.29 acres, 3.1 percent of the total project area.

Mixed Uses. A total of 77.4 percent of the total project area is proposed as mixed use. There are two categories of mixed uses, organized by villages:

- Retail/Commercial/Residential;
- Live/Work as described earlier.

3.5 VILLAGES-LAND USE PLAN

As discussed above, the project area is organized by villages. This section details the land use plan by each village for

existing setting, proposed changes in land use designation and zoning, development standards including parking, and the outcome.



A poor layout shopping center on the southwest corner of Tweedy Boulevard and Long Beach Boulevard; no landscape buffer between the sidewalk and the parking lot; the “No Cruising Zone” sign posted on the light pole indicates a certain traffic issue.



This commercial building is in the construction phase. The architectural style is appropriate; however, the building seems cut off from the public.



Helen Grace chocolate store is a popular Lynwood icon that has been open since 1956 on the corner of Martin Luther King, Jr. Boulevard and Long Beach Boulevard; the sign is a city landmark.

**3.5.1 DOWNTOWN VILLAGE I:
EXISTING BUSINESSES**

Downtown Village I, between Tweedy Boulevard to the north and Martin Luther King Jr. Boulevard to the south, is a combination of small businesses and some established larger businesses, ranging from neighborhood retail and motels to the larger Helen Grace Chocolate factory and store. The Specific Plan proposes to retain and strengthen these businesses. Existing and proposed land uses remain the same. See Figure 3.3 Proposed Downtown Village I Land Use Plan.

The focus in this village is on revitalization, encouraging businesses to renovate their existing buildings and construct new buildings in keeping with the architectural and landscape design guidelines specified for this village.

Existing Setting. This 27-acre village is mainly zoned C-3: Heavy Commercial with some R-3: High Residential, C-2: Light Commercial, and P-1: Parking (Table 3.B).

Table 3.B - Existing Land Use in Downtown Village I

Land Use	Acres
Commercial	19.91
Residential	6.78
Parking	0.31
Total	27.00

Current maximum density for all commercial land uses is a Floor Area Ratio (FAR) of 2:1 and 18 dwelling units per acre for Multifamily Residential under the City’s General Plan. These are not being amended in this Specific Plan.

The Plan recommends implementing a façade improvement program and sidewalk standards that support pedestrian-friendly sidewalks. The architectural design guidelines and landscape design guidelines establish the desired visual environment for tear down-rebuild and/or infill development. Further, the open space is being increased from 7 percent to 10 percent.

Zoning Change. As shown in Table 3.B, currently there are 19.91 acres zoned for Commercial and 6.78 acres zoned for Residential. No zoning changes other than open space requirement are proposed in this village.

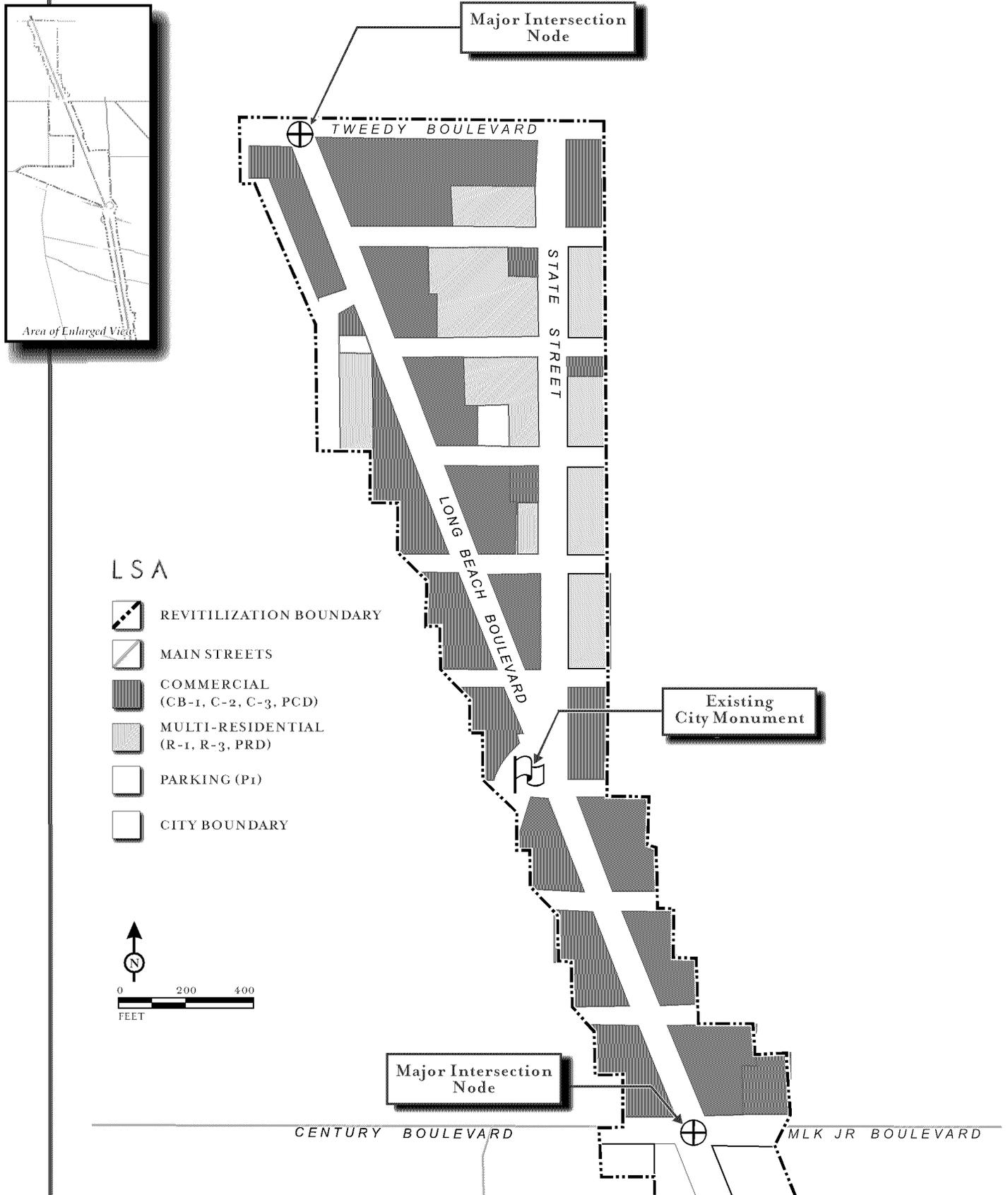
Development Standards. The development standards for this village will be amended from the existing standards for the front and rear setbacks; and courtyard and patio requirements (open space).

Development standards for Downtown Village I are enumerated below:

- Maximum Floor Area Ratio: Current maximum permitted of 2:1.
- Allowable Dwelling Units: Current maximum permitted of 18 dwelling units per acre.
- Dwelling Units: Permitted on second story and above.
- Lot Size: No minimum acreage, parcel consolidation recommended.
- Building Height: 75 feet maximum.
- Existing Landscape Area requirement: 7 percent of the site area.
- Courtyard and Patio requirement: Additional 3 percent to the above 7 percent for a total of 10 percent.

Long Beach Boulevard Specific Plan

Figure 3.3: Proposed Downtown Village I Land Use Plan



- Front Setback: Current requirement of 10 feet minimum, unless additional setback is required to develop a sidewalk of 18 feet.
- Side Street Setbacks: Current requirement of 10 feet minimum.
- Rear Setback: 5 feet minimum.

Existing Parking Code. The parking codes and requirements for Downtown Village I are enumerated below:

- Retail/Commercial component:
 - 1 space per 300 square feet of retail.
 - 1 space per 400 square feet of commercial.
 - 1 space per 100 square feet of restaurant.
- Multi-tenant retail/commercial:
 - 1 space per 100 square feet.
- Residential component:
 - Tandem parking permitted in garages.

Parking Requirements. Parking requirements shall remain the same as the current zoning.

Proposed Setting. This village will include the Spanish Colonial architectural style complementary to Downtown Village II. The arrangement of the commercial and residential uses within this village will place destinations within a reasonable walk, bike ride, or transit trip for local residents. It is necessary to provide appropriate landscape buffers, and creative design solutions, such as courtyard units. These are further detailed in Chapter 4.0 Architectural Design Guidelines. Chapter 5.0 specifies the Landscape Design guidelines for the Boulevard in general and specific to Downtown Village I.

3.5.2 DOWNTOWN VILLAGE II: MIXED USE AREA

The Downtown Village II is proposed as a mixed land use. It is bounded by Martin Luther King Jr. Boulevard to the north, I-105 to the south, California Avenue at the eastern section, and Bellinger Street to the west. Downtown Village II has a focus on mixed-use development with retail, commercial, civic, and residential uses adjacent to the transit-oriented area comprising a half-mile radius from the Green Line on I-105 as illustrated in Figure 3.4 Proposed Downtown Village II Land Use Plan. The proximity to transit is a vital element, as it has spurred developers' interest in mixed-use projects along Long Beach Boulevard.

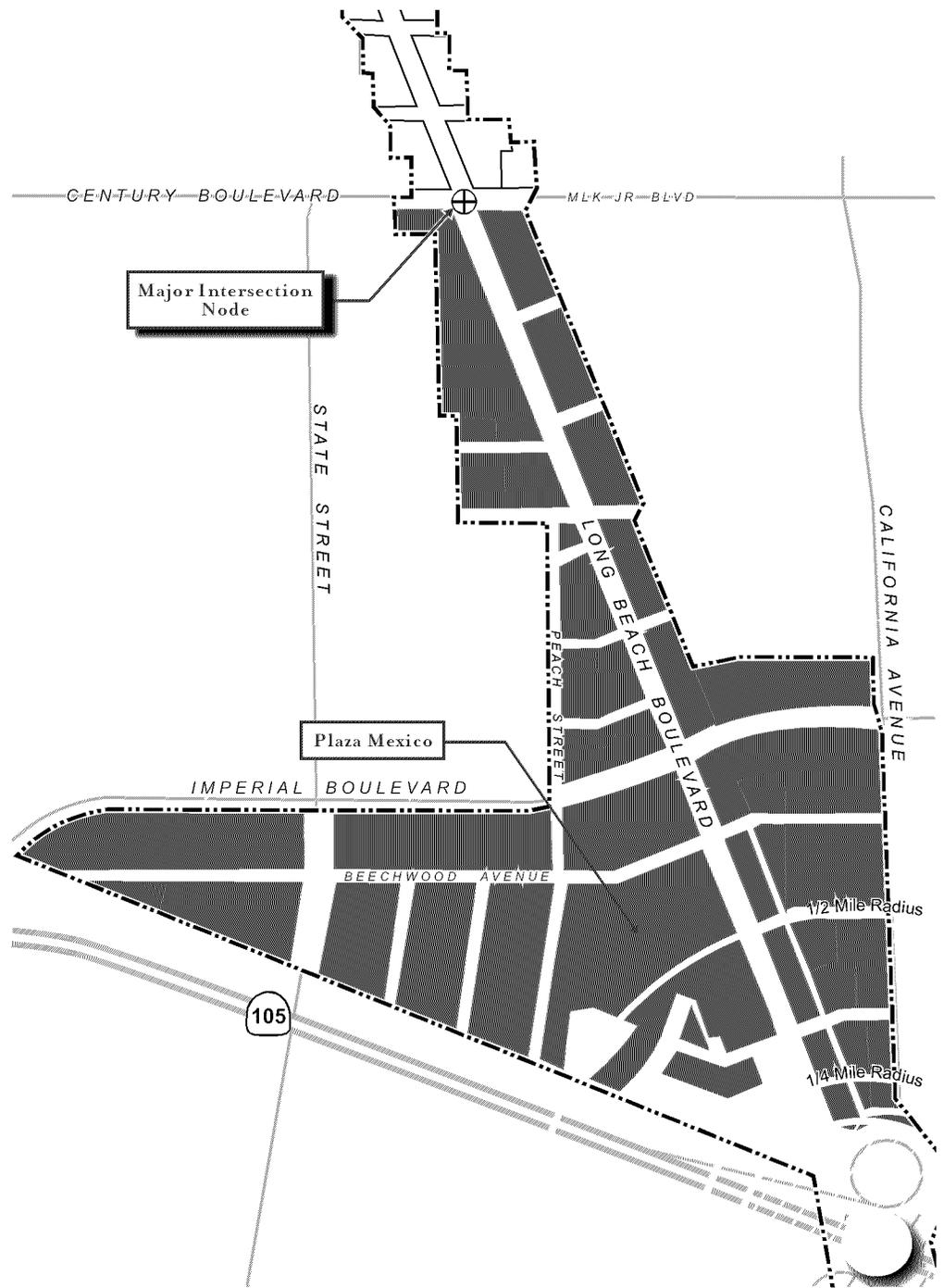
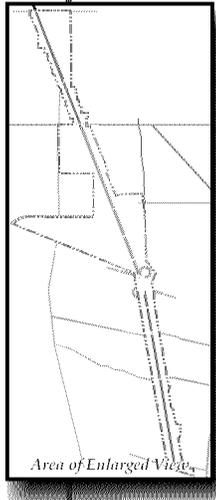
This village is likely to be a mid-term plan with emphasis on land uses that do not currently exist in the Specific Plan area, namely mixed use. Mixed use relates directly to TOD as it involves medium to high densities, planned pedestrian walkways, and multiple uses (particularly uses supporting transit ridership such as retail, child care, book stores, coffee shops, and amenities that improve public safety). Mixed uses within and adjacent to TOD create constant activity in diverse uses and housing.

The combination of ground-level pedestrian-oriented business, second-floor office space, and residential uses will create a highly livable area for the residents of the City, shoppers, and people who work on Long Beach Boulevard.

Many of these recommendations will ensure the area's success in becoming a safe and viable downtown for the City of Lynwood. The opportunities created in the

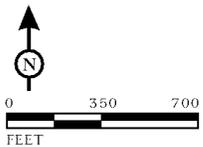
Long Beach Boulevard Specific Plan

Figure 3.4: Proposed Downtown Village II Land Use Plan



LSA

-  REVITALIZATION BOUNDARY
-  MAIN STREETS
-  MTA GREEN LINE METRO
-  TRANSIT VILLAGE GATEWAY
-  MIXED USE: RETAIL/COMMERCIAL/RESIDENTIAL
-  CITY BOUNDARY



village are also intended to capture some of the regional traffic on the corridor.

To the west of Long Beach Boulevard in Village II is Plaza Mexico, a 36-acre retail center built with an open air Mexican theme. This is the single largest development on the Boulevard in the City. Other parcels within this village would require parcel consolidation in order to propel mixed-use projects.

The Downtown Village II is envisioned to accommodate buildings that have restaurants, bookstores, shops with street fronts and smaller offices, and with residential uses in the vicinity of the Green Line. Specific uses that are civic in nature are recommended to be located in this village, namely, two police substations and a small City history museum. One police substation location is recommended just north of Plaza Mexico, and the second location near the Green Line station south of the freeway in Village III-Transit Village. As an example, a City history museum is envisioned to be about 1,000-2,000 square feet and positioned across from the existing U.S. Postal Service building or within any of the new mixed-use developments.

A parking structure built near Plaza Mexico will help to reduce the need for large surface parking, thereby allowing more land to be used for retail and commercial uses. The synergy created by mixing of uses will energize the pedestrian realm of Long Beach Boulevard while adding to the economic vitality of this area. The mixed uses will support the transit-oriented half-mile core of Long Beach Boulevard and I-105.

Existing Setting. The total acreage within the Downtown Village II is 81.40 acres,

which is currently zoned R3: Residential High Density, P1: Parking, C2: Light Commercial, C2A: Medium Commercial; C3: Heavy Commercial, and CB-1: Planned Business.



A typical retail on the corner of Long Beach Boulevard and Imperial Highway.

The current maximum density for commercial land uses is 2:1 FAR, and for Multifamily Residential it is 18 dwelling units per acre. The commercial designations in the Downtown Village II include a major retail component known as Lynwood's Towne Center. This center serves the consumer needs of the community and surrounding jurisdictions. Lynwood's Towne Center will continue to play a crucial role in the proposed Specific Plan and contribute to the anticipated increased revenue. The existing Downtown Village II includes the zoning designations shown in Table 3.C.

Table 3.C - Existing Land Use in Downtown Village II

Land Use	Acres
Commercial	59.21
Residential	11.32
Manufacturing	6.22
Parking	4.65
Total	81.40

The Plan recommends implementing mixed-use overlay/zoning over this village. The architectural design guidelines, landscape design guidelines, and site-specific recommendations establish the framework for the mixed-use projects that will revitalize the area over the coming years.

Zoning Change. At least 36 acres have already been built recently with the development of Plaza Mexico. Current zoning designation is mostly CB-1, Controlled Business with other commercial categories. The General Plan designation is Downtown Mixed Use. Zone changes will be required in this village to Mixed Use: Retail/Commercial/Residential. Additional changes for design guidelines, permitted uses, and development standards will enhance current and future businesses. Further, the open space is being increased from 7 percent to 10 percent.

Development Standards. The development standards for the mixed use trigger some critical amendments to existing standards.

Since this mix of uses is a market-driven issue, the Specific Plan does not specify the proportion mix. This will encourage developers to propose market-driven projects.

Development standards for Village II are enumerated below:

- Maximum Floor Area Ratio: 0.7 of the lot size.
- Maximum Allowable Dwelling Units: 30 dwelling units/acre.
- Dwelling Units: Permitted on second story and above.

- Lot Size: No minimum acreage, parcel consolidation recommended.
- Building Height: 190 feet maximum.
- Existing Landscape Area Requirement: 7 percent of the site area.
- Courtyard and Patio Requirement: 3 percent in addition to the above 7 percent for a total of 10 percent.
- Front Setback: Current requirement of 10 feet minimum will be increased on parcel basis to accommodate an 18-foot sidewalk.
- Side Street Setbacks: Current requirement of 10 feet minimum.
- Rear Setback: 5 feet minimum.

Existing Parking Code. The parking codes and requirements for Downtown Village II are enumerated below:

- Retail/Commercial component:
 - 1 space per 300 square feet of retail.
 - 1 space per 400 square feet of commercial.
 - 1 space per 100 square feet of restaurant.
- Multi-tenant retail/commercial:
 - 1 space per 100 square feet.
- Residential component:
 - Tandem parking permitted in garages.

Parking Requirements. Parking requirements need to be tailored to the land use mix for specific development projects. Demographic and locational factors and the evolving nature of transit ridership directly impact parking requirements for projects. It is recommended that proponents of mixed-use projects do a

parking demand analysis based on the land use mix and the generation rates in the context of the transit location.

The shared parking reductions and minimum requirements for mixed uses in transit areas have spurred many jurisdictions to amend their parking codes. As an example, the San Diego Municipal Code has reduced minimum parking requirements for nonresidential uses in transit areas to 85 percent of the minimum requirement for developments outside transit areas. Residential parking requirements in the transit area (very low income areas) have been reduced from 2 spaces per dwelling unit to 1.5 parking spaces per dwelling unit.



This building is located by the freeway off-ramp and is the entrance into the Downtown Village II; the “Lynwood” sign is hard to see and identify; the business phone number is larger than the address number.

Proposed Setting. This village will include the Italianate architectural style complementary to the Downtown Village I. The arrangement of the mixed uses within this village will place designations within a short walk, bike ride, or transit trip for local residents. Mixed-use buildings will have “vertical mixing” with ground-floor retail and upper-level housing and/or offices. It is

necessary to provide appropriate landscape buffers and creative design solutions such as courtyard units. These are further detailed in Chapter 4, Architectural Design Guidelines. Chapter 5 specifies the Landscape Design guidelines for Long Beach Boulevard in general and specific to Downtown Village II.



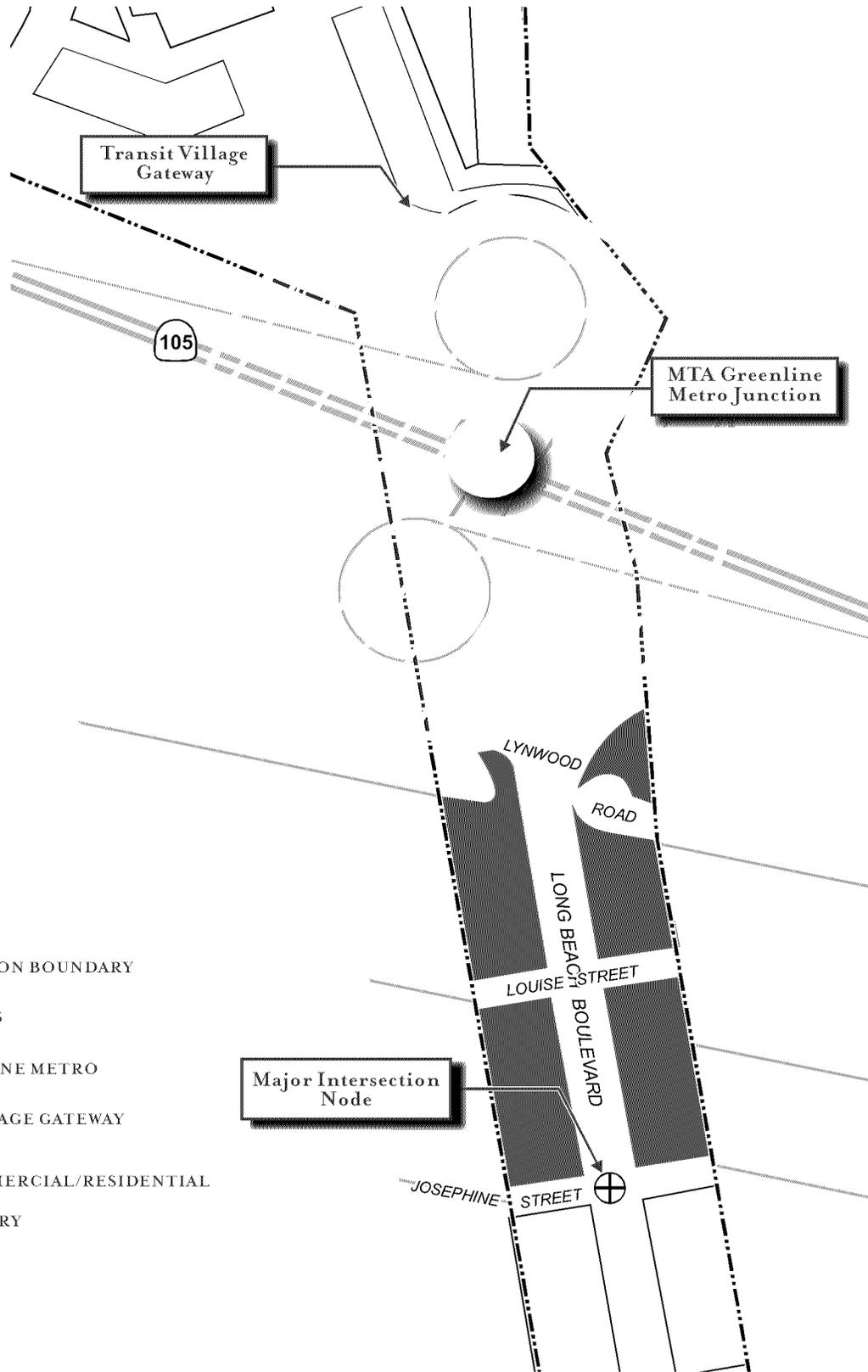
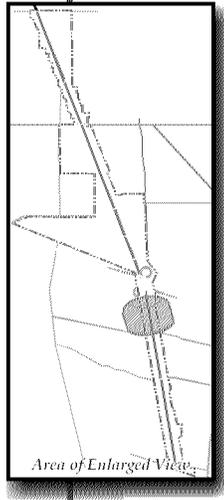
This corner parcel has an irregular shape, creating a site design challenge; temporary signs on this building are detracting; the landscape is too large for the building scale and hides the business.

3.5.3 VILLAGE III: TRANSIT VILLAGE

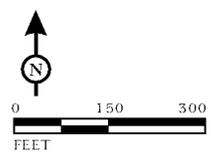
The Transit Village is a five-block, 4.67-acre village making it the smallest of the four sub-areas within the Specific Plan. Its objective is to accommodate a variety of commercial and public uses with emphasis on uses that support the transit system. Starting at the intersection of I-105 and Long Beach Boulevard and ending at Josephine Street, it is within 0.5 mile of the Green Line station, about 10 minutes on foot. The parcels lend themselves to nearby transit-related development. The five block location is found on Figure 3.5 Proposed Village III-Transit Village Land Use Plan.

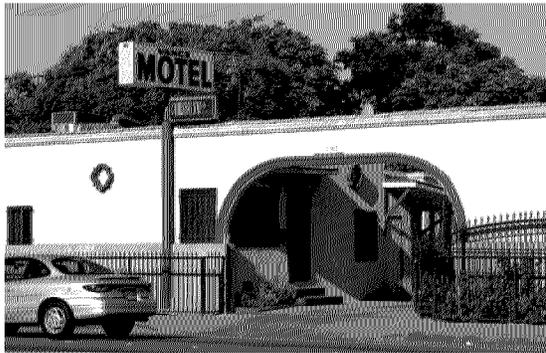
Long Beach Boulevard Specific Plan

Figure 3.5: Proposed Village III-Transit Village Land Use Plan

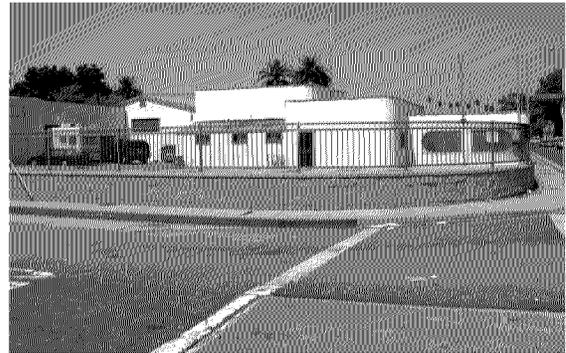


- LSA
-  REVITALIZATION BOUNDARY
 -  MAIN STREETS
 -  MTA GREEN LINE METRO
 -  TRANSIT VILLAGE GATEWAY
 -  MIXED USE:
RETAIL/COMMERCIAL/RESIDENTIAL
 -  CITY BOUNDARY





A typical motel along Long Beach Boulevard; the gates are not welcoming and convey the appearance of an unsafe area.



Rundown street with oil stains and patchwork paving; the building does not relate to the corner and there is no landscaping.



Single-story retail uses face most of Long Beach Boulevard in this area; non-matching, albeit good trees in the median; streetlights are not pedestrian scale.



Street has a pedestrian crossing sign, but no crosswalk; the building façade to the left has two different color paints, possibly because of graffiti cleanup; and the street median is devoid of any landscape.

Currently, the parcels have freeway-oriented uses such as a U-Haul depot, gas stations, and auto repair shops. From these blocks, the built character of Long Beach Boulevard steadily declines. It is critical for the regeneration purpose to target this village with projects that support transit and complement the land use development along the Boulevard.

Recommended land uses include retail/restaurants, food courts of various ethnic varieties, commercial uses, small offices, a police substation, and even parking structures should there be increased ridership on the Green Line. It is anticipated that development projects on these sites develop concurrently with projects in Downtown Village II.

Most of the land under the freeway area includes underutilized paved parking lots that provide a unique opportunity to expand appropriate village uses (see aerial photo on pages 22 and 25 in Chapter 2.0). Ideal uses include, but are not limited to, a Farmer's Market, various music/play events, and a skate park. Pedestrian walkways and sidewalks should safely connect this area to the adjacent villages to promote local businesses, safety, and easier access to the Green Line station.

Existing Setting. The 12 parcels currently located in the Transit Village are zoned for Medium Commercial (C-2A) use as shown in Table 3.D. In addition, the current maximum density for all commercial land uses within the City of Lynwood is 2:1 FAR and maximum allowable height of 75 feet. The C-2A zone allows for a retail center that will serve communitywide needs.

Table 3.D - Existing Land Uses for Village III-Transit Village

Land Use	Acres
Commercial	4.67
Total	4.67

Zoning Change. Within the proposed Transit Village, zoning will need to be altered to include an increase in FAR for commercial uses with a maximum of 0.7. Development of light commercial/retail with residential will alter the existing zoning to Retail/Commercial/Residential Mixed Use with additional customized uses so that the proposed uses can be accommodated within this district. Further changes for design guidelines and development standards will enhance the current and future businesses.

Development Standards. The development standards for this village will have a few changes to the existing standards such as FAR, permitted land uses, and rear building setbacks. The open space is being increased from 7 percent to 10 percent.

Development standards for the Transit Village are enumerated below:

- Maximum Floor Area Ratio: 0.7 of the lot size.
- Dwelling Units: 30 dwelling units/acre.
- Lot Size: No minimum acreage, parcel consolidation recommended.
- Building Height: no maximum.
- Existing Landscape Area Requirement: 7 percent of the site area.
- Courtyard and Patio Requirement: 3 percent in addition to the above 7 percent for a total of 10 percent.

- Front Setback: Current requirement of 10 feet minimum.
- Side Street Setbacks: Current requirement of 10 feet minimum.
- Rear Setback: 5 feet minimum.

Existing Parking Code. The parking codes and requirements for the Transit Village are enumerated below:

- Retail/Commercial component:
 - 1 space per 300 square feet of retail.
 - 1 space per 400 square feet of commercial.
 - 1 space per 100 square feet of restaurant.
- Multi-tenant retail/commercial:
 - 1 space per 100 square feet.
- Residential component:
 - Tandem parking permitted in garages.

Parking Requirements. Parking requirements need to be tailored to the land use mix for specific development projects. It is recommended that proponents of development projects do a parking demand analysis based on the land use mix and the generation rates in the context of the transit location.

Proposed Setting. Introduction of Italianate architecture style and design will merge the Transit Village with the Business Village. This distinct architectural style will produce a featured destination that will attract regional visitors and provide the community with an individual service. The intent of the Transit Village is to recapture underutilized lands, such as surface parking lots and vacant parcels, for a range of services. Other project goals include job-

creation for the community and an increase in sales tax revenue. In addition to commercial redevelopment, improvement of streets and sidewalks, public safety, and community expression is anticipated with the development of the Transit Village.

3.5.4 VILLAGE IV: BUSINESS VILLAGE

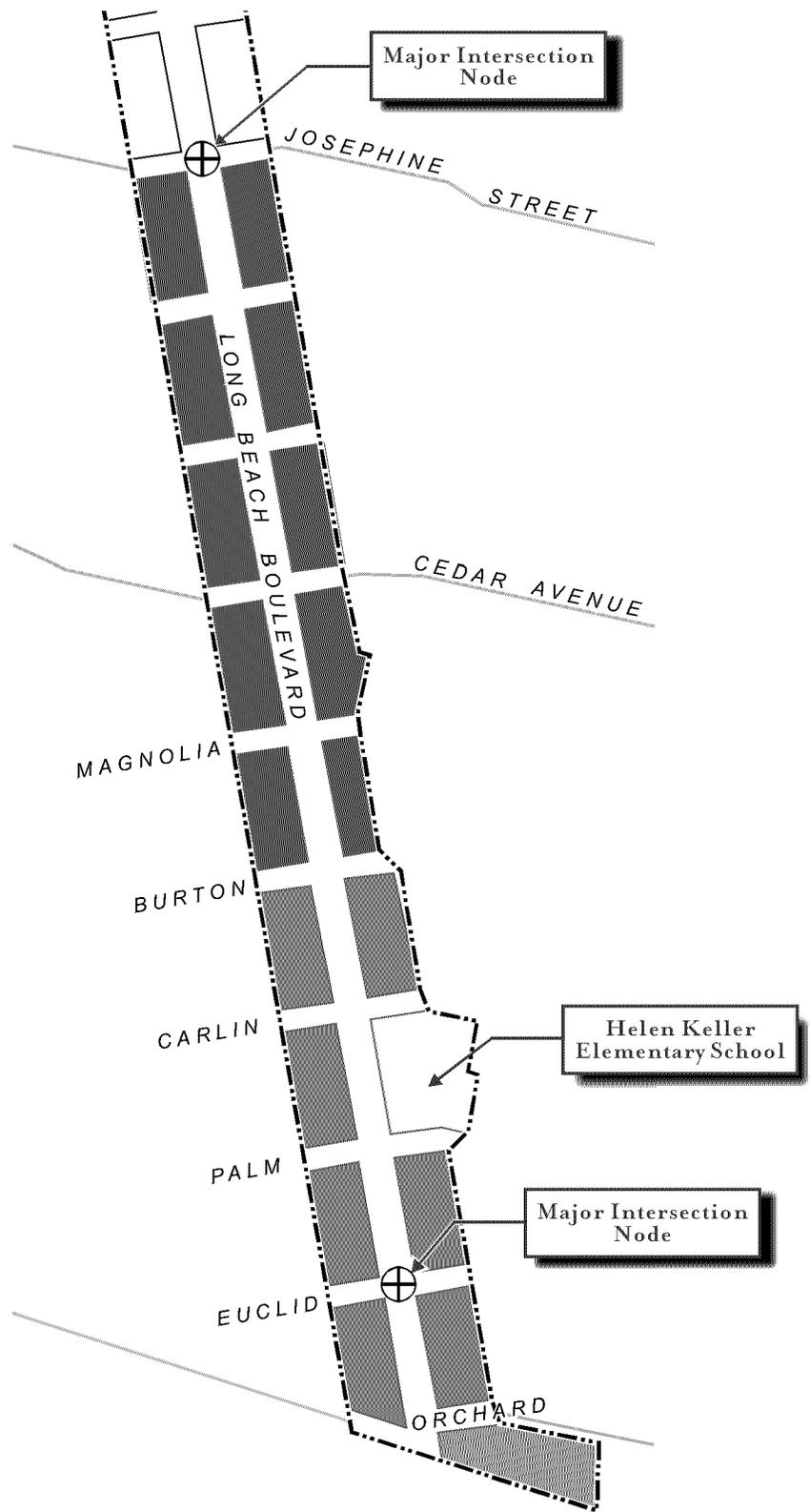
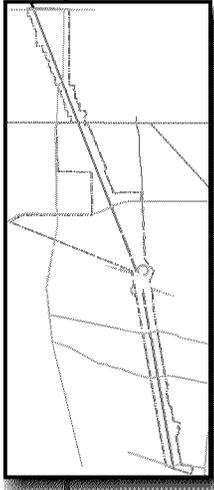
The Business Village is envisioned to provide improved commercial and office uses and live/work unit opportunities that can integrate into the existing commercial uses along Long Beach Boulevard.

The Business Village is adjacent to the Transit Village, creating an opportunity for the employees and consumers to use the Metro Rail. Pedestrian-friendly streets are highly encouraged to make the village successful. The boundaries of this village are Josephine Street to the north and Orchard Avenue to the south, and only include parcels facing Long Beach Boulevard as illustrated in Figure 3.6 Proposed Village IV-Business Village Land Use Plan.

Existing Setting. Currently, 103 parcels in the Business Village are zoned C-2A Medium Commercial; 4 parcels are zoned CB-1: Planned Business; and 20 parcels are zoned PF: Public Facilities (school). The current maximum FAR of C-2A is 2:1 with a height limitation of 75 feet according to the City's General Plan. The maximum FAR of CB-1 is 2:1 with a height limitation of 190 feet. The total acreage within the Business Village is 24.88 acres as shown in Table 3.E.

Long Beach Boulevard Specific Plan

Figure 3.6: Proposed Village IV-Business Village Land Use Plan



LSA

-  REVITALIZATION BOUNDARY
-  MAIN STREETS
-  MIXED USE:
RETAIL/COMMERCIAL/
RESIDENTIAL
-  MIXED USE:
LIVE/WORK
-  CONTROLLED BUSINESS
COMMERCIAL (CB1)
-  PUBLIC FACILITY
(SCHOOL, CITY, FIRE)
-  CITY BOUNDARY

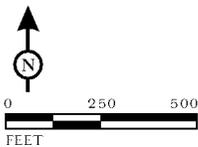


Table 3.E - Existing Land Uses for Village IV-Business Village

Land Use	Acres
Commercial	20.59
School	4.29
Total	24.88

Zoning Change. Zoning will need to be changed within the proposed Business Village to accommodate two mixed uses; (1) Retail/Commercial/Residential and (2) Live/work. There is an increase in Floor Area Ratio to 0.7 for Medium Commercial and Controlled Business uses. Street blocks between Josephine Street and Burton Avenue will be rezoned to Retail/Commercial/Residential mixed use. Street blocks between Burton Avenue and Orchard Avenue will be Live/Work mixed use. Additional changes for design guidelines, permitted uses, and development standards will enhance the current and future businesses and incorporate new live/work units south of Burton Avenue into the Business Village.

Development Standards. The development standards for this village will have a few changes to the existing standards. Open space requirement increases to 10 percent from the existing 7 percent.

Recommended development standards for the Business Village are enumerated below:

- Maximum Floor Area Ratio: 0.7 of the lot size.
- Allowable Dwelling Units: 30 dwelling units permitted.
- Dwelling Units: Permitted on second story and above for live/work units.

- Lot Size: No minimum acreage.
- Building Height: no maximum.
- Existing Landscape Area Requirement: 7 percent of the site area.
- Courtyard and Patio Requirement: 3 percent in addition to the above 7 percent for a total of 10 percent.
- Front Setback: Current requirement of 10 feet minimum.
- Side Street Setbacks: Current requirement of 10 feet minimum.
- Rear Setback: 5 feet minimum.

The following supplemental development standards are applicable to live/work units:

1. There is no limit on the number of live/work units in a project. A use permit will be required upon approval from the Planning Commission or person(s) in charge of processing.
2. Units must be a minimum of 1,400 square feet total, with a project average of 1,700 square feet. The living area may not exceed 70 percent of the total square footage and must be at least 460 square feet (not including bathroom or closet, which can be considered work space). Office space/work area must be at least 20 percent of the unit's living area. Up to 25 percent of work area can be dual-purpose.
3. The office working areas are required to have either separate and/or shared entries on the lowest level of the unit. Each unit shall be provided a primary entry from common areas such as hallways, corridors, and/or exterior portions of the building, such as courtyards, breezeways, parking areas, and public spaces.

4. Storage of hazardous material necessary for work activities in live/work occupancies may be stored in controlled areas per the current Uniform Building Code used by the City of Lynwood.
 5. Occupations that include flammable liquids, welding, open-flame work, or similar hazardous operations are not permitted in live/work occupancies.
 6. The residential portion of the live/work unit shall only be occupied by the individual whose professional occupation is established in the same unit and his or her family.
 7. No retail sales shall occur in a live/work space except for the products of the occupant's primary business.
 8. Noise levels generated by live/work spaces shall conform to the requirements of the City of Lynwood Municipal Code for residential use.
 9. All units must have kitchens and bathrooms that comply with the residential building code.
 10. A minimum of 7 percent of the site shall be used for landscape and 3 percent of each unit's square footage should be combined to be used in community facilities such as pools, spas, clubhouses, atriums, courtyards, barbeques, and shade structures.
 11. Delivery zones will be designated to accommodate delivery trucks such as UPS and FedEx.
 12. Work areas of the units shall be oriented toward the street, especially at ground level, where building entries, plazas, and windows should front onto the street.
 13. Units are allowed to have a mezzanine or mezzanine floor that is an intermediate floor placed within a room. A mezzanine normally must not exceed one-third of the area of the room into which it opens. It also must have 7-foot vertical clearance at all points above and below it; otherwise it is considered a story.
 14. Occupancy limits shall be a maximum of two occupants per 400 square feet with a maximum of 6 occupants.
- The following uses are permitted for live/work units:
- Apparel, including custom tailoring and sales of apparel, clothing, and other sewing products made on the premises.
 - Artists and craft/sculpture products, sales galleries, studios, and custom furniture.
 - Office including creative/tech-based offices and services.
 - Office including professional offices and services.
 - Photographer/photography studios.
 - Spas and personal care including nail salons and hair salons.
- Existing Parking Code.** The parking codes and requirements for the Business Village are enumerated below:
- Retail/Commercial component:
 - 1 space per 300 square feet of retail.
 - 1 space per 400 square feet of commercial.
 - 1 space per 100 square feet of restaurant.

- Multi-tenant retail/commercial:
 - 1 space per 100 square feet.
- Residential component:
 - Tandem parking permitted in garages.

Parking Requirements. Parking requirements need to be tailored to the land use mix for specific development projects. It is recommended that proponents of mixed-use projects do a parking demand analysis based on the land use mix and the generation rates in the context of the transit location.

Proposed Setting. Introduction of Spanish Colonial architecture style and design will merge the Business Village with the Transit Village. This distinct architectural style will produce a featured destination that will attract regional visitors and provide the community with an individual service. The 24.88-acre Business Village is bounded by Josephine Street to the north and Orchard Avenue to the south. The intent of the Business Village is to revitalize, rehabilitate, and/or recapture underutilized lands, such as surface parking lots and vacant parcels or buildings that will produce retail, office, and live/work units to complement Downtown Lynwood's emergence as a vibrant, multicultural urban village. Other goals of the project include job-creation for the community, reduced commuting, and an increase in sales tax revenue. In addition to commercial redevelopment, improvement of streets and sidewalks, public safety, and community expression are anticipated with the development of the Business Village.

Current Redevelopment Projects. There is only one project actively under consideration with the City's

Redevelopment Agency in the Business Village, which may include mixed use, retail and office development.

3.6 GENERAL PROVISION

A. Relationship to the Lynwood Zoning Ordinance. This Specific Plan augments the development regulations and standards of the Lynwood Zoning Ordinance. When an issue, condition, or situation occurs, which is not covered or provided for in the Specific Plan, the regulations of the Zoning Ordinance that are most applicable to the issue, condition, or situation shall apply. In the event that the provisions of the Specific Plan are in conflict with the Zoning Ordinance, the provisions of the Specific Plan shall prevail.

B. Interpretation. The Community Planning Director or his or her designee shall have the responsibility of interpreting the provisions of the Specific Plan, except that the Community Planning Director may refer the matter to the Planning Commission. All such interpretations shall be in written form and permanently maintained. Any person aggrieved by such an interpretation may make a formal request in writing to the Community Planning Director that such interpretation be reviewed by the Planning Commission. Such appeal is subject to applicable fees and processing requirements.

C. Enforcement. The City shall enforce the provision of this Specific Plan and all the applicable codes of all governmental agencies and jurisdictions in such matters including, but not limited to, building, mechanical, fire, and electrical codes pertaining to

drainage, wastewater, public utilities, subdivisions, and grading.

D. Definitions. Words, phrases, and terms not specifically defined herein shall have the same definition as provided in the City of Lynwood Zoning Ordinance. Definitions of words, phrases, and terms as used in this Specific Plan are included in the Glossary in the Appendix.

3.6.1 CUSTOMIZED USES

Each village area is defined in precise terms to ensure that the range of permitted and conditionally permitted uses respects the intent of the plan and the conditions encountered in each area of the site. Table 3.F outlines the permitted (P), conditionally permitted (C), and prohibited (N) uses within the Long Beach Boulevard Specific Plan. This list was derived from existing zoning regulations to allow greater or lesser flexibility in some areas of use and to customize land use requirements in response to site characteristics, potentials, and limitations. This tailored use list is specifically aimed at stimulating investment that will evolve into a pattern of uniformly higher value and quality than has prevailed in the past. The prohibited uses figure significantly in the sub-areas so as to avoid potential conflicts between uses within and adjacent to the site.

3.6.2 DETERMINATION OF UNLISTED USES

The Planning Director, upon written request, or the Planning Commission, upon referral by the Planning Director, shall be allowed to determine whether a use not specifically listed as a use that is principally permitted or conditionally

permitted in the Specific Plan area should be given such designation based upon a similarity to uses already allowed.

3.6.3 SETBACKS AND BUILD-TO LINES

The standards established for each village shall apply to new non-residential and mixed-use development within the Specific Plan area. Setbacks are measured from the property line. Where ground-level retail uses are present, setbacks may be increased up to 12 feet for outdoor seating or sales (e.g., product display). Features such as overhangs, porticos, balconies, loggias, arcades, covered (non-enclosed) bicycle parking, pergolas, and similar architectural features placed on the front (street-facing) side of the building shall be allowed within the setback.

The standards established in each village shall apply to new residential development within the Specific Plan area. Features such as front porches, overhangs, porticos, balconies, loggias, arcades, covered (non-enclosed) bicycle parking, pergolas, and similar architectural features placed on the front (street-facing) side of the building shall be allowed within the setback.

3.6.4 LOT AND DWELLING SIZE

New multiple-family development shall maintain a minimum lot area of 20,000 square feet. Notwithstanding the minimum lot area requirement, a new development shall be allowed a minimum net lot area of 1,250 square feet per unit.

Table 3.F – Customized Use List

Use Category	Downtown Village I	Downtown Village II	Transit Village	Business Village
RESIDENTIAL				
Assisted Living	N	C	N	N
Boarding, rooming and lodging facilities	N	C	N	N
Duplex	P	P	N	N
Mobile Home Park	N	N	N	N
Multiple-Family	P	P	N	N
Residential Care facilities (nursing home, rest home, convalescent home, or elderly congregate care)	N	C	N	N
Residential Housing for the Elderly	C	C	N	N
Single-Family Attached	P	P	N	N
Single-Family Detached	P	N	N	N
COMMERCIAL SERVICES AND RETAIL				
Ambulance service	N	N	N	N
Animal sales and services	P	P	N	N
Auto/recreational rentals/sales, new and used	P	N	N	N
Banks and Savings and Loans	P	P	P	P
Boarding, rooming and lodging facilities	C	C	C	C
Catering Services	P	P	N	P
Cemetery	N	N	N	N
Children's Day Care Services	C	C	C	N
Funeral and mortuary services	P	N	N	N
Grooming, indoor only	P	P	P	P
Massage parlors, escort service	N	N	N	N
Nurseries	C	N	N	N
Pawn Shops/Check Cashing	C	N	N	N
Personal Services	P	P	P	P
Recycling facilities	N	N	N	N
Retail Sales & Service	P	P	P	P
Retail sales and supply	P	P	P	P
Secondhand stores	C	N	N	N
Self Service Car Wash	N	N	N	N
Service Stations	P	N	N	N
Storage	N	N	N	N
Swap meets	C	N	N	N
Tattoo parlors	N	N	N	N

Table 3.F – Customized Use List

Use Category	Downtown Village I	Downtown Village II	Transit Village	Business Village
Vehicle equipment sales	C	C	N	N
Vehicle repair facilities	C	C	N	N
Veterinary services	P	P	N	C
COMMERCIAL RECREATION/ENTERTAINMENT				
Adult Businesses	N	N	N	N
Amusement arcades	C	C	C	C
Bowling alleys	C	C	N	N
Dance halls	C	C	N	N
Golf Courses	N	N	N	N
Gyms, fitness centers < 2,000 square feet	P	P	P	P
Gyms, fitness centers > 2,000 square feet	P	P	N	N
Indoor sports/recreation	P	P	N	N
Live theater venues	C	C	C	N
Movie theaters	C	C	C	N
Pool/billiard halls	C	C	N	N
Tennis, swimming clubs	C	C	N	N
COMMUNITY AND HUMAN SERVICES				
Drug abuse and alcohol recovery centers	N	N	N	N
Emergency shelters	N	N	N	N
Hospitals, Medical Clinics	N	N	N	N
Public utility/communication facilities	N	N	N	N
Schools, public/vocational/trade	C	C	N	C
Transitional housing	N	N	N	N
Educational facilities	P	P	N	C
Facilities for the mentally disordered, handicapped, dependent, or neglected persons	N	N	N	N
EATING AND DRINKING				
Bar, cocktail lounge, tavern	C	C	C	N
Bona-fide restaurant	P	P	C	C
Dinner Theatre	C	C	N	N
Drive through	C	C	N	N
Food court in/out door	P	P	P	P
Live entertainment/dancing (incidental)	C	C	C	N
Nightclub	N	C	N	N
Take out only	C	C	N	C
With fast food or take out	P	C	N	N

Table 3.F – Customized Use List

Use Category	Downtown Village I	Downtown Village II	Transit Village	Business Village
FOOD AND BEVERAGE SALES				
Food markets, grocery stores	C	C	N	C
Food, mini-mart convenience stores	C	C	C	C
Liquor and wine stores	C	C	N	N
Supermarket	C	C	N	N
MIXED USE				
Live/Work	N	N	N	P
Lofts	P	P	C	P
Mixed Uses, vertical/horizontal	C	P	P	P
PROFESSIONAL OFFICE				
Administrative and other offices	P	P	P	P
Medical/Dental, massage therapy, and chiropractic	P	P	N	P
Real Estate, Insurance	P	P	N	P
Temporary Office Uses	C	C	N	C
TOURIST SERVICES				
Bed and breakfast	C	C	N	C
Hotel and motel	C	C	N	N
Recreational vehicle park	N	N	N	N
Single-room occupancy	N	N	N	N
Ticket agency	P	P	P	P
Travel agency	P	P	P	P
TEMPORARY AND INTERIM USES				
Outdoor Uses (except plaza-related uses)	C	C	C	C
Outdoor vendor carts	N	N	N	N
Christmas Tree, Pumpkin, similar lots	C	C	N	N
Parking Lot Sales	N	C	N	N
Street/Craft Fair Farmers Markets	C	C	N	N

Table Notes:

P = Permitted C = Conditional Use Permit N = Not Permitted

- Existing non-conforming uses shall be subject to the provision of Chapter 25-15.2 of the City of Lynwood Zoning Ordinance.
- Since Downtown Village II provides the greatest level of flexibility of all the Villages identified in the Specific plan, it will be especially important to ensure that any development or combination of uses in this area meets with the intent of the Specific Plan. The findings identified in the Infrastructure, Implementation, and Maintenance Plan of the Long Beach Boulevard Specific Plan shall be used to confirm that uses proposed consist of the appropriate mix and are compatible with uses permitted in and adjacent to Downtown Village II.

3.6.5 DENSITY

Density can be the result of many people wanting to be in the same geographic area. There are three main factors that make a place special and desirable by offering the type of vibrant street life not found in many typical American cities. Two prerequisites for urban vitality are a pedestrian-friendly streetscape and a mix of uses. The third essential component is density. Shops, restaurants, and cultural institutions cannot survive without a critical mass of people nearby to support them. As residential density increases above certain thresholds, these services and amenities become viable. Population density is a key determinant of transit viability and the likely success of TOD projects. Current population within one mile of the intersection of I-105 and Long Beach Boulevard is 43,000 according to the City's official website.

New non-residential and mixed-use development within the Specific Plan area shall achieve minimum FARs as stated in the Table 3.A and in each village development standard. New residential uses within the Specific Plan area shall achieve densities according to Table 3.A and in each village development standard.

Density Bonus Program. "Density Bonus" means a minimum density increase of at least 25 percent over the otherwise Maximum Residential Density.

For all new development and the conversion or alteration of existing development, density bonuses shall conform to State density bonus law (Government Code Section 65915). Section 65915, as amended by Chapter 842, Statutes of 1989, required all cities and counties to adopt density bonus

ordinances. The law provides that local governments shall grant density bonuses of at least 25 percent, plus an additional incentive(s) or equivalent financial incentives, to housing developers who agree to construct at least: 20 percent of the units affordable to lower income households, 10 percent of the units affordable to very low-income households, or at least 50 percent senior citizen housing. Condos are eligible if at least 20 percent of units are restricted to households of moderate income. The City may offer an Equivalent Financial Incentive in lieu of granting a Density Bonus and an Additional Incentive(s). The value of the Equivalent Financial Incentive shall equal at least the land cost per dwelling unit savings that would result from a Density Bonus and must contribute significantly to the economic feasibility of providing the Target Units pursuant to this chapter. The law applies to developments of five or more units.

3.6.6 BUILDING HEIGHT

For all new development and the vertical alteration of existing development, building heights within the Specific Plan area shall conform to the development standards set for each village.

3.6.7 LOT COVERAGE

New development within the Specific Plan area shall achieve lot coverage of not more than that defined in the development standards for each village.

Features such as front porches, overhangs, porticos, balconies, loggias, arcades, covered (non-enclosed) bicycle parking, pergolas, and similar architectural features placed on the front (street-facing) side of

the building shall be exempt from the lot coverage requirement.

3.6.8 BUILDING FRONTAGE AND FAÇADES

In order to support the pedestrian-oriented environment within the Specific Plan area, building frontages onto streets and open spaces shall be maximized.

Minimum requirements for windows and openings are detailed in Chapter 4.0, Architectural Design Guidelines.

3.6.9 BUILDING ENTRY

If a building is adjacent to a transit street or a major pedestrian access way, at least one main building entry shall be oriented to the adjacent transit street and/or major pedestrian access way. A pedestrian way shall be provided from the building entry to the transit street or major pedestrian access way.

To allow for their use, residential porches shall have a minimum clear depth of six feet and shall be a minimum of 50 square feet.

3.6.10 RESIDENTIAL AMENITIES

All multiple-family developments with four or more dwelling units shall provide usable common open space for passive and active recreational uses. Usable open space areas shall not include public or private rights-of-way; vehicular parking areas; areas adjacent to or between structures less than 15 feet apart; required building setback areas, private patios or yards; or areas having a slope greater than 3:1. Usable open space can include roof decks

accessible to all residents in the development.

Within multiple-family residential projects, a minimum of 100 square feet of usable private open space shall be provided.

All common open space associated with multiple-family developments with four or more dwelling units shall include, at a minimum, 7 percent of the site for landscape and 3 percent of each unit's square footage to be combined for community facilities uses (e.g., pools, spas, clubhouses, atriums, courtyards, barbeques, and shade structures).

Additional amenities may include, but are not limited to, tot lots with play equipment, benches, and court game facilities. The types of amenities shall be dependent upon the nature of development and shall be approved by the reviewing authority. Further details and conceptual photos of courtyards are in Chapter 4.0 Architectural Design Guidelines.

All new development within the Specific Plan area shall incorporate an element of public art equivalent to 1 percent of total building cost. Guidelines for incorporating public art are included in Chapter 4.0 Architectural Design Guidelines.

3.6.11 SIGN REGULATIONS

New signage within the Specific Plan area shall conform to the standards stated herein and with the requirements established by Subsection 25-33.4 (sign standards by zoning district) of the City's Municipal Code and City Ordinance Numbers 1972 §17-122; 1025, §11; 1506, §2.

In the event of a conflict between the standards given herein and City's Municipal Code and Zoning Ordinances, the TOD standards shall prevail.

Signage shall not reduce clear sidewalk width to less than 8 feet. Opaque signage shall not reduce permeability of street-fronting windows to less than the minimum clear window requirement within Chapter 4.0 Architectural Design Guidelines.

General Sign Standards. One address number no more than 6 inches vertical shall be attached to the building in proximity to the principal entrance or at a mailbox.

Non-residential corner buildings shall have their addresses attached to the building, clearly visible from the intersection, and have 12-inch vertical lettering.

One blade sign for each separate business entrance may be permanently installed perpendicular to the façade. Such a sign shall not exceed a total of 6 square feet unless otherwise approved by the Planning Director.

Further details for building signage are in Chapter 4.0 Architectural Design Guidelines.

3.6.12 PARKING REQUIREMENTS PER LAND USE TYPE

For new development within the Specific Plan area, the number of required parking spaces (on-street and off-street) shall be based on a parking demand analysis, especially for shared parking proposals.

Parking space calculations resulting in a small fraction shall require a whole space.

Additional Innovative Measures. No parking is required for uses under 2,500 square feet if parking is available within 600 feet.

Allow curbside parking spaces and shared spaces within 600 feet to be counted in the requirement to allow further reductions in off-street parking for mixed-use projects and those with good transit access.

Curb parking along residential uses will count toward visitor parking requirements only. The minimum linear frontage is 22 feet per parallel parking stall and 9 feet for head-in and angled parking.

Off-Street Parking. Off-street parking shall be located to the rear and/or interior of a lot such that its visibility from a street shall be minimized.

At-grade, aboveground, or belowground parking structures shall be permitted. At-grade parking structures shall have a minimum frontage as outlined in Chapter 4.0 Architectural Design Guidelines.

Surface parking lots shall be placed between the structure and a side or rear lot line. Where a lot fronts onto two or more streets, parking shall be located accordingly:

- Along the street with the least amount of commercial activity; or
- Along the street with least amount of pedestrian activity if the lot is located along two or more commercial streets with equal amounts of commercial activity.

A wall or fence between 4 and 6 feet in height shall separate parking lots from abutting residential uses with landscaping as stated in the Landscape Plan. Walls and fences shall take on the character of residential uses.

Podium parking used in any development shall be partially submerged with the above-grade height not to exceed 5 feet. Landscaping shall be used to screen exposed openings.

Garages associated with single-family residential uses, whether attached or detached, shall be set back at least 10 feet behind the primary front façades of the buildings they serve. The primary front façade shall comprise at least 50 percent of the overall width of the primary residence and the 10-foot setback shall not be measured from projections such as bay windows and porches, but from the façade of the wall which encloses the building.

Parking structures, including structures developed in conjunction with the Transit Center shall adhere to the same setbacks, frontage, and height requirements as buildings within their respective neighborhood zones.

Parking structures in commercial and mixed-use areas fronting on a street shall include ground-floor uses to create a pedestrian-supportive interface with the abutting sidewalk. If a use cannot be provided, appropriate landscaping shall be used as screening.

Bicycle parking will primarily be on the sidewalks along the side streets that connect to Long Beach Boulevard.

3.6.13 LOCATION OF VEHICLE ACCESS

Conflicts between pedestrians and vehicles entering and exiting parking lots shall be minimized. Access from pedestrian-oriented streets shall be avoided unless no other reasonable access is available, such as in lots with a single-street frontage and no alley.

Where alleys are present, driveways leading to parking lots, and loading and service areas shall be accessed from the alley. Lots with more than one street frontage and no alley shall locate vehicular access along the street with the least amount of pedestrian activity. All loading and service drives shall be of a depth that prevents loading and service vehicles from obstructing the sidewalk and roadway.

Access driveways shall not dominate the street frontage, and driveway widths shall be minimized to reduce their presence along the street.

Where feasible, driveways shall be consolidated within the single lot and shared with adjacent properties to minimize their encroachment upon sidewalks.

Shared driveway agreements shall be utilized where possible for shared parking, and loading and service areas.

To avoid encroaching upon sidewalks and creating uneven pedestrian surfaces, driveway slopes shall be located between the roadside edge of the sidewalk and the curb.

Loading and Service Area Location. Loading, service, and refuse areas shall be located at the interior of the lot and screened from view with walls, trellises,

planting, berms, or by integration into the design of the building.

Walls shall not exceed 6 feet in height. Solid walls shall be landscaped to soften their appearance and shall be made of finished materials to match the primary building. Decorative elements, variation in materials, and articulation shall be used.

4.0 ARCHITECTURAL DESIGN GUIDELINES

4.1 DOWNTOWN VILLAGE I - SMALL BUSINESSES AND VILLAGE IV - BUSINESS VILLAGE

■ SPANISH COLONIAL ■

These design guidelines apply to new construction and the façade improvement of the existing small businesses located in Downtown Village I.



Order in alignment of the windows.

4.1.1 ARCHITECTURAL STYLES

The architectural style recommended for the buildings in Village I is Spanish Colonial.

Spanish Colonial Style. Spanish colonization of California from mid-eighteenth through early nineteenth century gave rise to a new exotic and ornate architectural style. In 1769, the King of Spain, in order to firm up his claims on California, assigned people to build missions. These missions were built with Spanish architectural ideals and details influenced by Moorish, Gothic, and Renaissance architectural styles. The different Spanish architectural styles developed later, taking their cue from these early Spanish missionaries and Spanish Colonial buildings and adding their own regional flavors. Identifying

features of this style include low-pitched roofs, asymmetrical façades, thick stucco walls with recessed openings, arched window openings, and elaborate grille and tile work.

4.1.2 MASSING AND ARTICULATION

• *Varied and Articulated Buildings*

1. The base of the building should establish a human scale for the pedestrians and visually join the building to the ground.
2. The main body of the building shall be architecturally subdivided into segments to reduce the height and bulk. This can be achieved through variation of roof height, wall breaks, vertical and horizontal projections, color, and material.
3. The top of the building should be a sloped roof or enhanced parapet wall according to the architectural style of the buildings.
4. Varied massing and multi-planed roofs are required to achieve varied building articulation.
5. Window header height shall be maintained to unify the different building masses and convey a harmonious street scene.
6. Building corners at major intersections shall be architecturally emphasized and have tower-like elements higher than the rest of the building.

• *Building Walls*

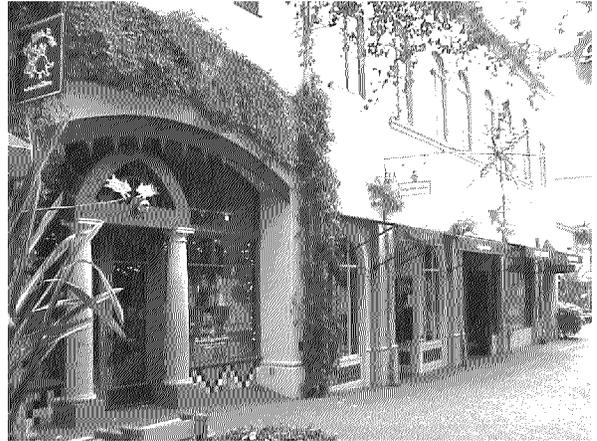
1. Building surfaces more than 30 feet in length should be relieved by

providing horizontal or vertical changes to provide visual interest.

2. Long blank walls shall be avoided. Blank building wall areas more than 150 square feet shall be treated architecturally with stucco recesses, balconies, railings, shutters, corbels, pilasters, tile insets, or other three-dimensional elements acceptable architectural projections.
3. In order to incorporate interactive pedestrian streetscape façades, 70

percent of the first story for retail/commercial walls shall have glazing with windows, entrances, or openings.

4. The building floors are required to be articulated with the use of recessed entries, insets, canopies, awnings, arcades, trellis, balconies, recessed windows, corbels, etc.
5. All building façades visible from public streets and public areas shall be architecturally treated.



Recessed openings.



Articulated façade.



Architecturally treated side elevation.

4.1.3 STREET FAÇADES

1. The main entrances for businesses shall be oriented toward Long Beach Boulevard. The main entry shall provide a “sense of entry” to that business. Secondary entries shall enter from a building parking area or connecting pedestrian pathways.
2. Retail and commercial uses are encouraged to decorate their storefronts with different materials and colors original to the architectural style.
3. Windows and doors shall be placed to facilitate natural surveillance.
4. The glass façade of businesses, such as doors, windows, and display glass, may not be boarded or covered by paint, large banners, or other large opaque materials.

4.1.4 REAR OR SIDE ELEVATIONS

1. Rear and side elevations with secondary entrances to the retail, office, or residential units shall be architecturally treated.
2. Service and storage areas shall be screened.

3. Business signage and lighting shall be provided to identify the businesses for the patrons.
4. Awnings, display windows, and other architectural elements shall be used to enhance the elevation and appeal to the customers.
5. Side and rear elevations facing residential areas shall be complementary to that use in its architectural details, lighting, signage, etc. The businesses shall not have light spillage onto adjoining parcels.
6. Loading and unloading in the service areas shall be located away from the main and public view streets and residences whenever feasible and well screened.
7. Mechanical units shall be concealed from public view by landscaping or, if roof mounted, by decorative parapets and/or other architectural elements.

4.1.5 COURTYARDS

1. Plazas and courtyards are encouraged to be incorporated in the buildings.
2. Environmental factors such as sun, prevailing winds, and humidity shall

be considered while designing courtyards and plazas in order to provide proper ventilation to the buildings.

3. Shops, restaurants, offices, and other active spaces should open to the courtyard instead of blank walls.
4. Courtyards and plazas should be designed in a way that is accessible from the main street and have an unimpeded line of sight from the public right-of-way.



A courtyard.

4.1.6 SPANISH COLONIAL ARCHITECTURAL ELEMENTS

1. The roof shall be low-pitched.
2. The roof materials shall be fire-resistant using concrete, clay 'S,' or barrel tile.
3. Main gable roofs shall have roof pitches of 3:12 to 4:12.
4. The roof overhangs shall be 12" to 18" wide with exposed wood rafter tails.
5. Exposed rafter tails, notched or rolled stucco fascia is recommended.
6. Decorative gable end vents are required for gables facing the main streets.

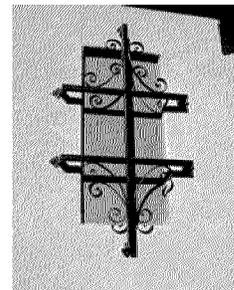
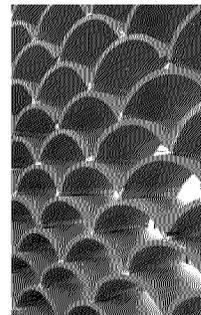
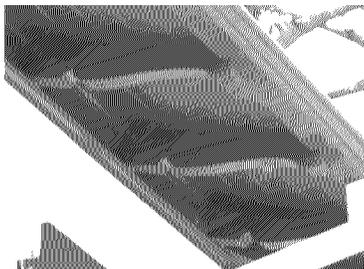
7. One or more prominent arches shall be placed above doors or principal windows.
8. Entry elements in the form of colonnaded arcades with elaborate masonry arches supporting the roof are recommended. Columns supporting the porches can also be of heavy timber often with a bracket above or square Greek revival columns.
9. Juliet balconies with wrought iron railing and brackets are encouraged.
10. The cantilevered balconies can be supported by exposed wood, precast concrete, or stucco corbels.
11. The railing for the balconies shall be of wood or metal.
12. Windows shall have simple trims with header and sill; variations of sill including sloping and sculpted stucco sill are recommended.
13. Feature windows and doors shall be recessed to convey a thick wall appearance.



For awning, projections, building signage, garages, and building lighting, please refer to 4.2 Village II and Village III design guidelines.

- *Materials and Color*

1. Stucco on the exterior wall shall be smooth to light sand finish or light dash finish.
2. Walls shall use stucco, wood, and wood-like materials.
3. The main stucco color shall be of light earth tone colors per attached color palette.
4. Roofs shall be terracotta to brown hues per attached color palette.
5. Bright accent colors and tile colors are encouraged.



Spanish Colonial Color Palette

(The following SHERMAN WILLIAMS or equal colors should be used)

FASCIA/TRIM/WOOD ACCENTS

6062-6108
6076-6101
6083-6087
6089-6090
6095-6097
6102-6104
6109-6111
2806-2808
7008
7009
7012

STUCCO (FIELD COLOR)

6053
6059/6060
6067
6077-6078
6084-6087
6105-6108
6117/6123
6121
2022
2805
2803
7006
7008
7009
7012

ACCENT COLORS

(WROUGHT IRON/SHUTTERS/ENTRY/MULLIONS)

2837
2843
2810
2811
2803
2802
2801
6229
6222
6214
6215
6181
6139
6132
6118
6027

ROOF ('S' TILE)

Terracotta Blends
Brown Blends
Tan/Terracotta Blends
Brown/Terracotta Blends
Red/Terracotta Blends
Tan/Brown/Terracotta Blends

4.2 DOWNTOWN VILLAGE II & VILLAGE III - TRANSIT VILLAGE

▣ ITALIANATE ▣

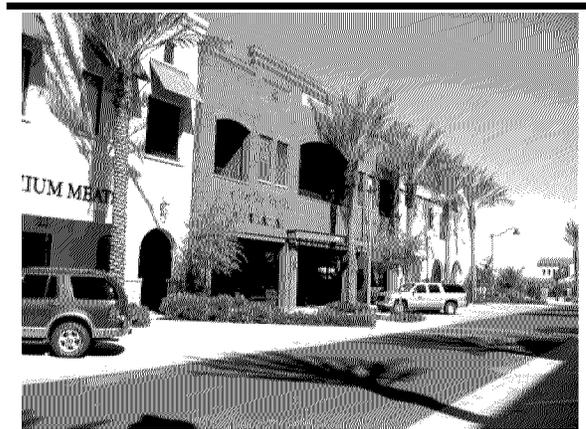
The mixed-used buildings in the Long Beach Boulevard Specific Plan are multistory buildings that contain both retail/commercial and residential uses within Downtown Village II, while Village III - Transit Village includes a mixed use of only retail and commercial.

4.2.1 ARCHITECTURAL STYLES

The architectural style recommended for the buildings in Downtown Village II and Village III - Transit Village is Italianate, reflecting the history and culture of the City. This style is visually compatible with Spanish Colonial in Downtown Village I and, when adjacent, creates a diverse and visually interesting street scene.

Italianate Style. The Italianate style began as part of the picturesque movement, a shift away from a more formal direction in art and architecture. The new period style generated classic informality, with traditional towers, as models for Italian-style villas.

The identifying features of this style include low-pitched mostly hipped roof, overhanging eaves with decorative brackets underneath, tall windows sometimes with an arched or curved top, horizontal belt courses, corner quoins, pediment entry elements, and feature tower elements.



Varied architectural styles.

4.2.2 MASSING AND ARTICULATION

• *Varied and Articulated Buildings*

1. The buildings shall have clear distinct base, main body, and top sections. The base should establish a human scale for pedestrians and visually join the building to the ground. The base shall be defined by a change of material, color, or architectural elements such as arcades and/or projecting structural elements. The base shall be visually divided from the main body with the help of a horizontal belt course, awnings, and series of openings, projections, or cornices. The main body of the building shall be

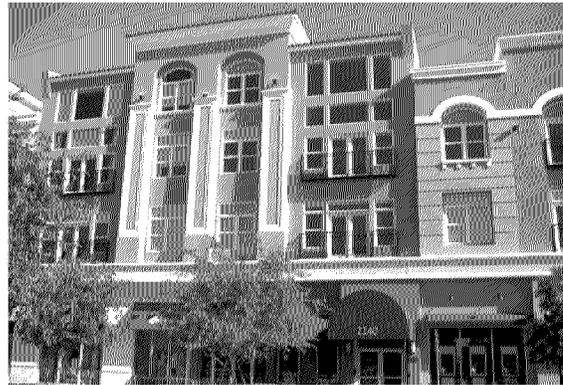
architecturally divided to reduce the height and mass. The top of the building shall be a sloped roof, cornice, or treated parapet wall according to the architectural style of the buildings.

2. Varied multilevel rooflines defined by tops of parapets or eaves are required to achieve vertical building articulation.
3. The main pedestrian entrance for the upper stories shall be clearly visible from the main street. It shall be architecturally treated with recessed entry, private courtyard, lobby, vestibule, or other architectural entry treatment.
4. Header heights shall be maintained to unify the different building masses and convey a harmonious street scene.
5. Building corners at major intersections shall be architecturally emphasized and have tower-like elements higher than the rest of the building.

Façade Improvement

Building façades have great influence on the overall character of Long Beach Boulevard. Enhancing the building façade will also improve the image of the business and provide a sense of community pride for the inhabitants.

Any building façade improvement within the Long Beach Boulevard Specific Plan should follow the design guidelines of its respective village.



Articulated building façade.



Varied roof heights and building massing.



Distinct retail base.



Elevation

• *Building Walls*

1. Long blank walls shall be avoided. In order to achieve an interactive pedestrian streetscape, 70 percent of the first-story façade wall shall have glazing, either with windows, entrances, or openings. Blank building wall areas more than 300 square feet shall be treated architecturally with balconies, railings, shutters, corbels, pilasters, tile insets, or other three-dimensional acceptable architectural projections. Façades shall be architecturally subdivided into 25' to 50' vertical modules to create a traditional "Main Street" appearance.
2. The first-floor businesses are required to be articulated with the use of recessed entries, insets, canopies, awnings, arcades, etc. On the residential units above, architectural features such as balconies, porches, corbelled projections, wrought iron window balconies, and other three-dimensional architectural elements shall be provided to achieve building wall articulation.
3. Individual wall mechanical units are allowed but need to be flush to the

exterior with decorative grille and have dimensional trim finished the same as the surrounding architecture.

4. All building façades shall be architecturally treated.



Covered patio area



Residential balcony projections.



Residential entrance from the main street.



Covered arcade at the base.

4.2.3 STOREFRONTS

1. The main store entrance shall be oriented toward Long Beach Boulevard.
2. Retail and commercial uses are encouraged to decorate their storefronts with different materials and colors original to the architectural style.
3. Windows and doors shall be placed to facilitate natural surveillance.
4. The glass façades of the businesses, such as doors, windows, and display glass, may not be boarded or covered by paint, large banners, or other large opaque materials.

4.2.4 REAR OR SIDE ELEVATIONS

The rear and side elevations shall face the side streets, parking areas, or the residential areas.

1. Rear and side elevations, considered as secondary entrances to the retail or residential units, shall be architecturally treated.
2. Service and storage areas shall be well screened and sited. Loading areas shall be located away from the main and public view streets whenever feasible and well screened.
3. Business signage and lighting shall be provided for the patrons to identify the stores.
4. Awnings, display windows, and other architectural elements should be used to enhance the elevation and appeal to the customers.
5. Side and rear elevations facing residential areas shall be complementary to that use in its

architectural details, lighting, signage, etc.

6. Mechanical units shall be concealed from public view by landscaping or, if roof-mounted, by decorative parapets and/or other architectural elements.



Articulated building façades.



Articulated rear elevation.



Service street elevation.



Building courtyard-private open space with projecting balconies overhead.

4.2.5 COURTYARDS

1. Internal building courtyards are required to be incorporated into the building design.
2. Courtyards shall be private or semipublic gathering spaces serving as common open area for the residential complex, restaurant outdoor dining, or urban informal gathering space.
3. The building courtyards can be formal gardens or urban spaces surrounded with loggias for shaded seating and set with fountains and lush landscaping elements.
4. Environmental factors such as sun, prevailing winds, and humidity shall be considered while designing courtyards in order to provide proper ventilation to the buildings.



Rich vegetation within a courtyard to add shade and privacy.



Public courtyard/plaza area.



Building courtyard/Private open space.

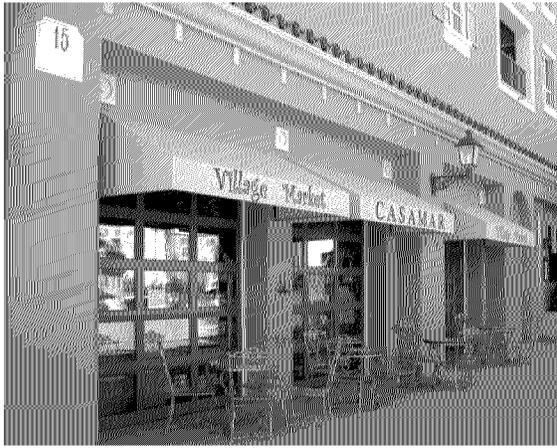
4.2.6 AWNINGS AND PROJECTIONS

1. The size of the awnings and canopies shall be proportional to the building. Long continuous awnings and/or canopies are discouraged.
2. The first-floor awnings and/or canopies must be between 8' and 10' above the surface where they project and can project a maximum of 5' into the right-of-way.
3. The style of the awning and/or canopy shall complement the style of the architecture of the building. The storefront awning should be fabric mounted over a metal structure or frame or permanent architectural awning material derived from the building architecture to which it is attached.

4. Internally illuminated awnings and canopies with or without signage should not be used.



Storefront building awnings.



Building awning providing shade to outdoor seating.

4.2.7 BUILDING SIGNAGE

Building signage on Long Beach Boulevard shall be pedestrian-oriented and shall be proportional to the building size. The signage shall create interest and contribute to the lively “Main Street” experience.

1. The signage shall be architecturally compatible to the building style.
2. Illuminated box signs, roof signs, and pole signs are not allowed.

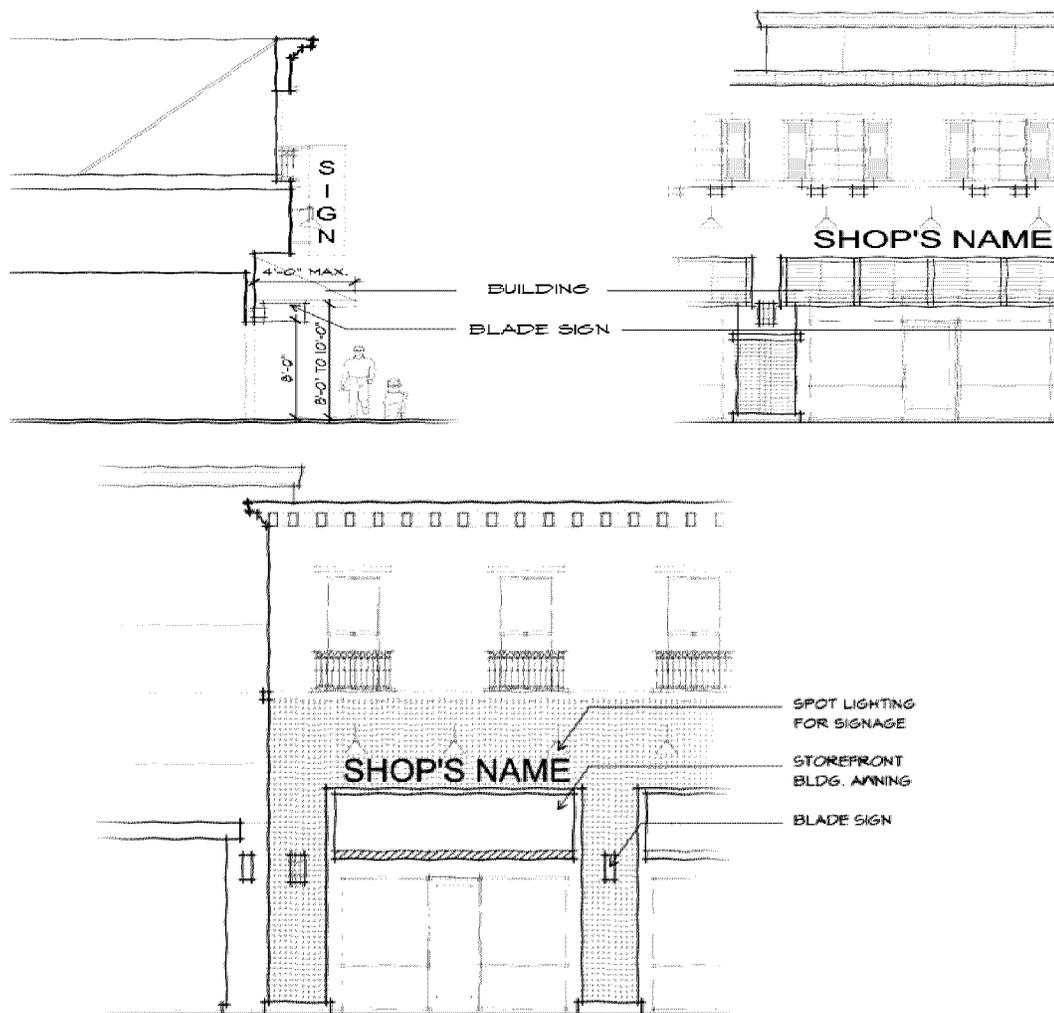


Storefront oriented toward “Main Street.”

3. Building-mounted marquee signage intended for the motorists should be designed as part of the architectural façade of that building. It should not obscure or hide architectural features of the building.



Pedestrian-oriented blade signs.



4. Building-mounted marquee signage intended for the motorists should be designed as part of the architectural façade of that building. It should not obscure or hide architectural features of the building.
5. Blade signs must provide 8' clearance from the finished grade level and can project a maximum of 2'-6" from the face of a building wall to the front of the sign.
6. The structural supports for the signage should match the architecture of the building to which it is attached.
7. Signage is not allowed on the sloping portion of the building awning or canopies.
8. Cloth and paper signs are not allowed. Signs must be made of permanent materials such as metal, wood, ceramic, stone, etc. and shall complement the material and style of the building.
9. Window signage cannot be more than 2 square feet.
10. Individual backlit letters are permitted for signage.



Building signage.

11. A comprehensive signage program shall be submitted to the City of Lynwood for approval.

4.2.8 BUILDING LIGHTING

The main purpose of building lighting is to provide safety and illumination around the building.

1. The lighting design shall be compatible to the architectural style of the building and shall be integrated with the architectural design of structures.
2. There should be no sky globe effect or light spillage onto other properties.
3. Building signs illuminated above or below by spotlights are permitted.
4. Flashing or blinking lights are not permitted.



Building lighting.

4.2.9 GARAGES

1. Podium parking and parking entrances facing streets shall be architecturally treated.

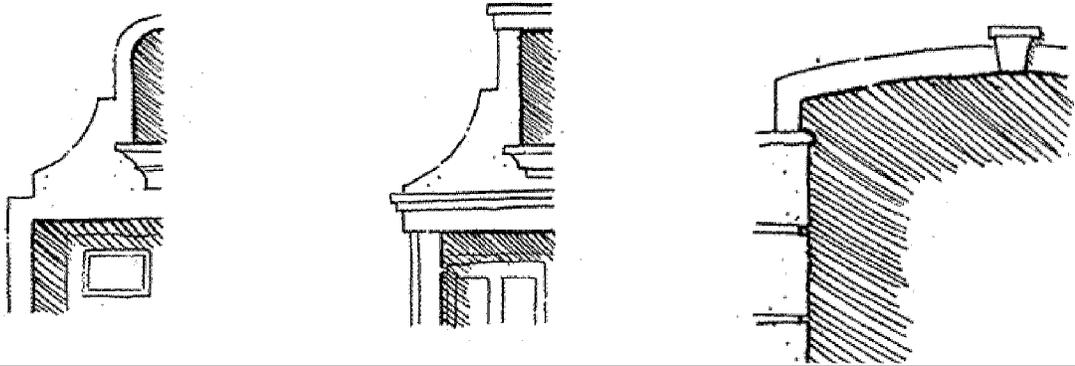
2. Garage entrances shall be recessed from the façade of the building.
3. Vehicular entrances for the podium parking structure shall be architecturally treated and make an entry statement for the vehicles.

4.2.10 ITALIANATE ARCHITECTURAL ELEMENTS

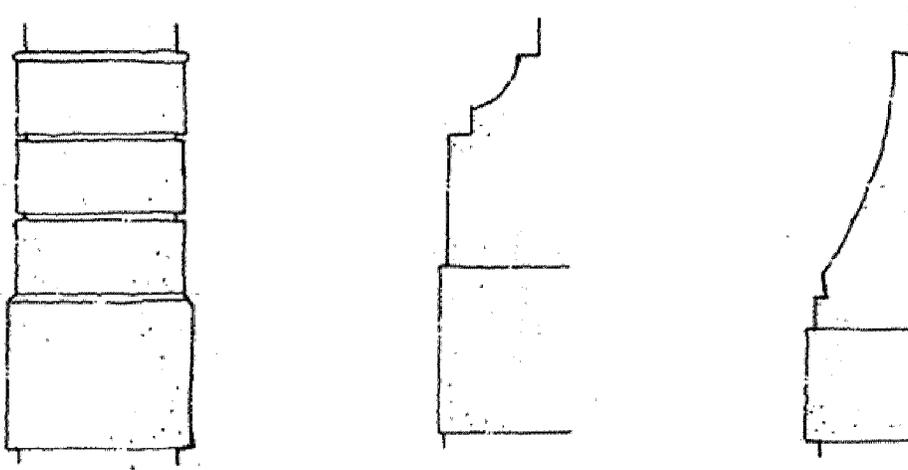
1. The roof shall be flat or low-pitched with concrete flat clay tile, concrete 'S,' or clay tile.
2. The eaves shall have an overhang of 12" to 36".
3. Eaves with decorative brackets detailing underneath are recommended.
4. Arched arcade elements at the commercial front and covered balconies are recommended to enhance the architectural style of the building.
5. The windows shall be typically tall and narrow proportionally for the residential and business uses, which form the main body of the buildings.
6. Windows can be in pair or triple configurations.
7. Window sashes shall be most commonly with one or two-pane glazing.
8. Some feature windows shall be arched above and have U-shaped crowns, often with brackets.
9. Rectangular windows shall have simple window trims or pediment crowns.
10. Door tops shall be in same shape as the windows with large-pane glazing.
11. Entry porches shall be covered, commonly supported by square posts with beveled corners.
 - *Materials and Colors*
 1. Walls shall be of stucco with stone or precast concrete accent elements.
 2. The stucco shall be light-colored, off-white to medium hues.
 3. Roof fascias, windows, and door trim shall be of stucco, stone, cast concrete, or wood
 4. Fascia shall match the stucco color or be a lighter or darker version of the stucco color.
 5. Sloped roof shall have flat or 'S' tile roof. The roof tiles shall be terracotta, red, brown, or grey blends.
 6. Balconies shall have decorative wrought iron railing, concrete precast balusters, or stone balusters.
 7. Precast elements are recommended for entries to residential complexes, parking structures, and other commercial entrances.



Architectural elements.



Entry elements



Pilasters with quoins

Italianate-Color Palette

(The following SHERMAN WILLIAMS or equal colors should be used.)

FASCIA/TRIM/WOOD ACCENTS

7004
7012
7029
7036
7050
6112/6113
6126/6127
6133/6134
6140/6141
6154/6155

ACCENT COLORS

(WROUGHT IRON/SHUTTERS/ENTRY/MULLIONS)

2802
2811
2819
6006
6034
6076
6090
6118
6124
6153
6174
6207
6215
6243
6265
6327
6990
7076

STUCCO/PRECAST CONCRETE

ELEMENTS (FIELD COLOR)

7029
7036
7050
6058-6060
6113-6116
6127-6130
6134-6137
141-6143
6155-6157
6169-6171
6162-6164

ROOF ('S' TILE)

Terracotta Blends
Brown Blends
Grey Blends
Brown/Terracotta Blends
Brown/Green Blends
Tan/Brown/Terracotta Blends

5.0 CIRCULATION AND LANDSCAPE PLAN

5.1 CIRCULATION PLAN

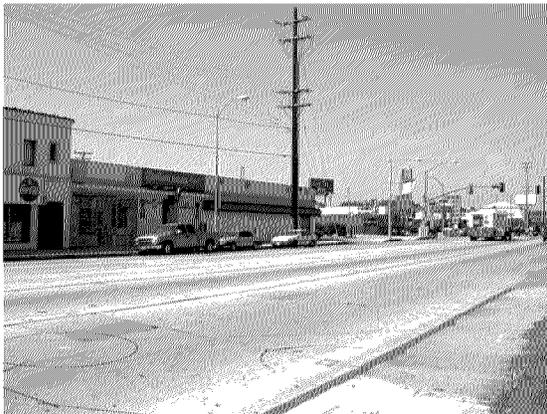
5.1.1 VISION FOR LONG BEACH BOULEVARD

The vision for the circulation and landscape plan for Long Beach Boulevard is to link the corridor integrally within the City and create a vibrant and viable downtown.

5.1.2 EXISTING CONDITIONS

As described previously, Long Beach Boulevard presents myriad street conditions, both for traffic and pedestrian movement. It exhibits typical conditions of a major thoroughfare of a built-out city. It absorbs regional traffic as well as local traffic within the cities.

Long Beach Boulevard, under the maintenance of City of Lynwood Public Works Department, is a busy street. The width of the Boulevard ranges from 82 feet to 110 feet along the 30 blocks within the City. The sidewalk space is approximately 9-10 feet wide on most blocks.



Current Street Condition



Busy Intersection

5.1.3 METRO GREEN LINE

The addition of a Green Line station in 1995 on Interstate 105 brings access to transit for the population in the corridor. The east-west Green Line has 14 stations that connect LAX to communities from Norwalk to El Segundo. It is a corridor with a high level of activity and the addition of transit investment was the harbinger for the real estate changes that have been ongoing in the last few years. The Green Line and its existing synergy with the land uses along Long Beach Boulevard are critical in setting the land use programmatic development for transit-oriented development (TOD).



Green Line Station at Long Beach Boulevard

5.1.4 CIRCULATION DEVELOPMENT OBJECTIVES

In keeping with the goals of the project and also principles of TOD, the Circulation Plan aims to create a pedestrian-oriented environment while maintaining the vehicular traffic on Long Beach Boulevard by providing:

- Space for pedestrians and bicyclists within a highly active urban thoroughfare;
- Buffers to protect pedestrians and bicyclists from traffic; and
- Safe pedestrian crossings at the street crosswalks, particularly for access to the transit station.

Besides the sidewalk along the Boulevard, the proposed plan emphasizes the creation of pedestrian ways with “civic” spaces, such as plazas in Downtown Village II and Village III-Transit Village. This is illustrated in previously referenced Figure 3.6 and is intended to become a pedestrian network away from the Boulevard.

5.1.5 CIRCULATION PLAN ELEMENTS

The circulation plan advocates uniform sidewalk width and travel lanes along the length of the Boulevard.

Angled parking is to be provided along the Boulevard, designed amidst landscape planters. Angled parking in clusters of 5 to 7 stalls is recommended.

There are exposed utility lines along parts of the Boulevard. Under-grounding of utilities is to be undertaken in such instances.

The City has identified a need to prepare and implement a signage package that includes design for street names, color scheme, and logos. Further, traffic poles and other appurtenances will also be redesigned as part of the Circulation Plan.

5.2 LANDSCAPE PLAN

5.2.1 LANDSCAPE CONCEPT

Long Beach Boulevard is envisioned to have a consistent streetscape design with emphasis on major intersections in order to create a memorable corridor with distinct identity.

5.2.2 LANDSCAPE DEVELOPMENT OBJECTIVES

Based on the primary goal of maintaining a visual cohesiveness and unity on the Boulevard, the landscape plan embodies following objectives:

- Providing an identity plant palette along the Boulevard;
- Marking the entrance and major intersections with enhanced landscape elements;
- Recommending a well-designed sidewalk; and
- Embodying local cultural elements in public art.

5.2.3 LANDSCAPE PLAN ELEMENTS

The circulation and landscape elements are planned cohesively and are described in the following section as streetscape elements.

5.3 STREETScape ELEMENTS

Street design can be considerably enhanced by several components (elements). These include sidewalk space; street intersections; design treatments at major street intersections or nodes; pedestrian crossings; and street landscaping. These elements add to the character of the street and are further described in this section.

5.3.1 SIDEWALKS

The ideal sidewalk conditions would allow for a planting buffer between street and sidewalk, a street furniture area, walking/strolling spaces and window shopping/browsing area as explained below. The sidewalk for Long Beach Boulevard is divided into four distinct zones: the Edge Zone (1), the Furnishing Zone (2), the Throughway Zone (3), and the Frontage Zone (4) as shown in Figure 5.1.

- **Edge Zone (1).** This is the transition from the roadway to the sidewalk, usually defined with a curb.
- **Furnishing Zone (2).** This is the next zone in the sidewalk. It provides a buffer between pedestrians and street traffic. It generally includes trees, tree lawns, phone booths, parking meters, trash cans, traffic signal cabinets, fire hydrants, bicycle racks, seating, etc.
- **Throughway Zone (3).** This is intended for pedestrian travel only. Ideally, it should be clear of obstacles, including driveway aprons. The minimum width of this zone is 4 feet for Americans with Disability Act (ADA) accessibility with

allowance of 5 feet at least every 200 feet.

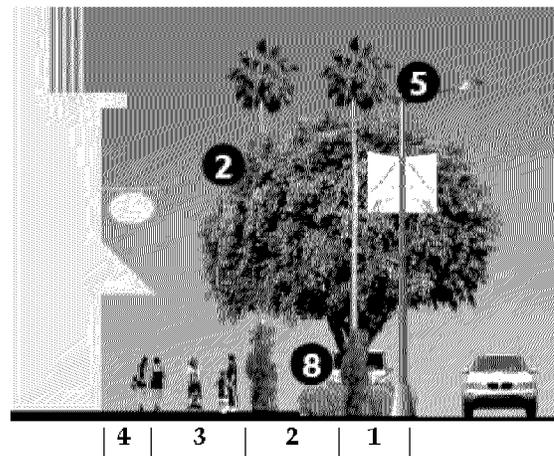


Figure 5.1: Sidewalk Zones

- **Frontage Zone (4).** This includes the portions of the sidewalk nearest to property lines. This is the zone in which pedestrians interact with shop displays, slowing down to window shop or enter and exit buildings.

In absence of a Furnishing Zone, the Frontage Zone also accommodates some of the furnishing such as lights, benches, vertical water elements, and trash cans among other outdoor furniture.

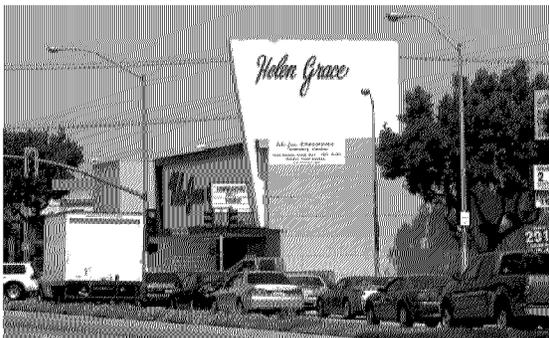
In order of emphasis, the mixed-use Downtown Village II and Village III-Transit Village shall have at least 18 feet of sidewalk space for pedestrian circulation.

Downtown Village I and Village IV-Business Village are planned to have smaller building envelopes along the sidewalks and these villages should ideally have 18 feet of sidewalk space.

5.3.2 DOWNTOWN VILLAGE I DESIGN GUIDELINES

The following design guidelines apply to Downtown Village I:

- **Minimum sidewalk width:** 18 feet.
- **Planting width:** 3-4 feet.
- **Sidewalk Material:** PCC.
- **Planting Materials:** Mexican Fan Palm or Canary Island Date Palm, Fichus (specimen tree), Shrub (hedge), and Bougainvillea (vine). Figures 5.2 through 5.5 illustrate the landscape plan in Downtown Village I.
- **Street Furnishings:** Street lighting, outdoor seating, bicycle racks, trash receptacles, and other amenities are provided on the sidewalk.
- Street lighting will also carry banners designed to announce special civic or cultural events.
- Water elements should be implemented into the streetscape without acting as a barrier to pedestrian circulation. Small fountains or vertical water falls attached to the building façade would work best due to the limited sidewalk space.



Helen Grace Chocolate Factory

- **Street Intersections:** The major street crossing at Tweedy Boulevard at Long Beach Boulevard is enhanced by pedestrian crossing that is paved with stamped or colored concrete as illustrated in Figure 5.3.
- **Landmark Element:** A landmark element in the form of an archway or gateway is planned across the

intersection of Tweedy Boulevard (entrance to the City) and Long Beach Boulevard.

The intersection of Martin Luther King Jr. Boulevard and Long Beach Boulevard is an important one for the City, marked by the presence of Helen Grace, the premium chocolate factory and factory store (since 1944). The recommendation for this important intersection is to either enhance the marquee of the Helen Grace building with its signage or introduce the City gateway monument. The existing sign monument at Tenaya Avenue and Long Beach Boulevard is to be removed.



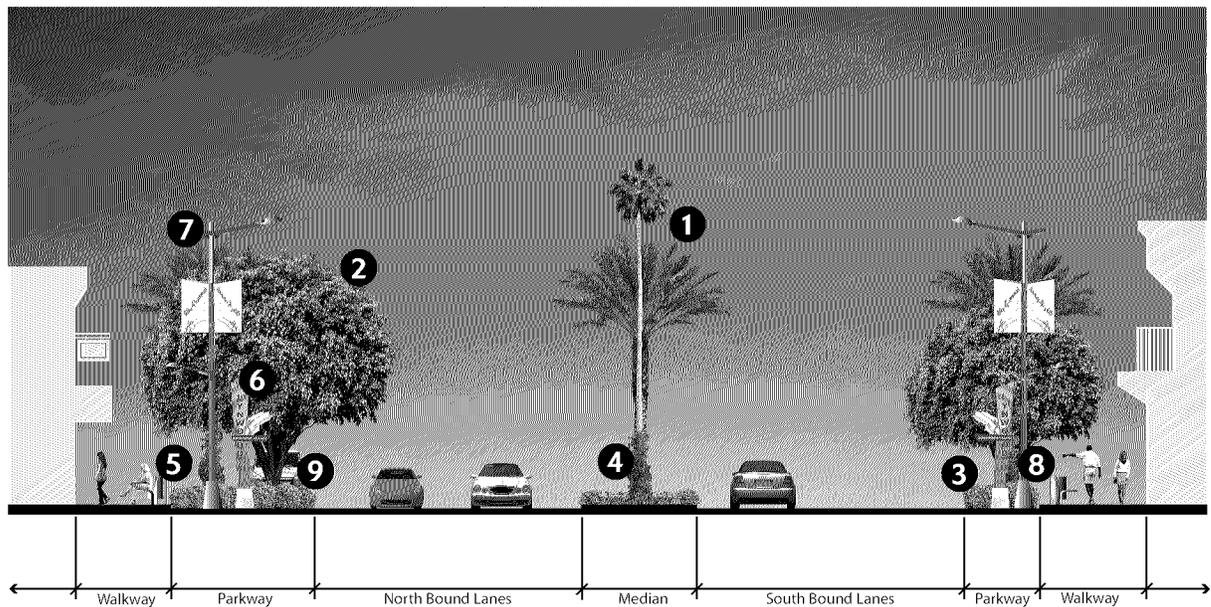
The current welcome sign for Lynwood does not stand out.



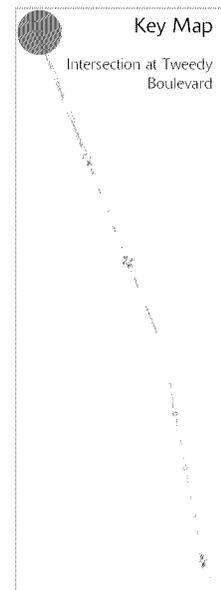
Current Lynwood welcome sign and clock tower monument at Tenaya Avenue

Long Beach Boulevard Specific Plan

Figure 5.2: Intersection at Tweedy Boulevard

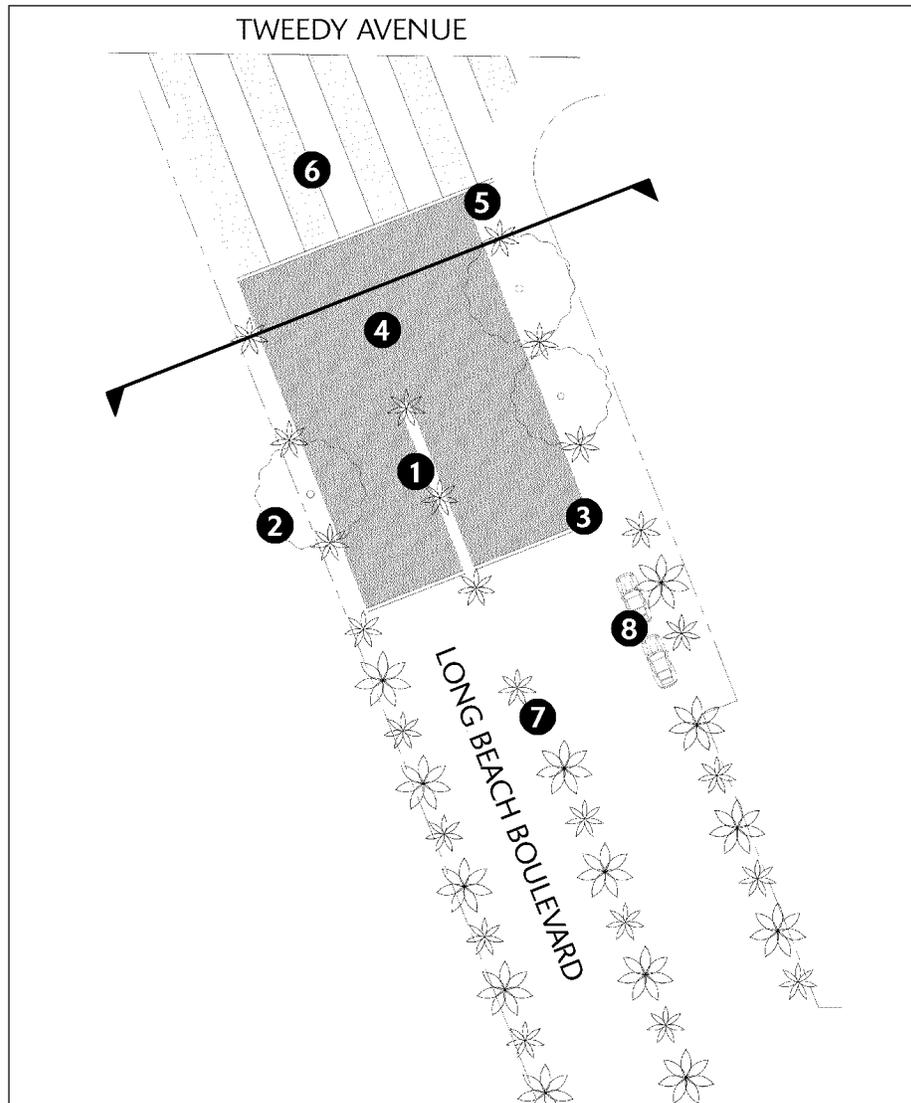


- ① Mexican Fan Palm or Canary Island Date Palm
- ② Ficus - Specimen Tree
- ③ Shrub - Hedge
- ④ Bougainvillea - Vine
- ⑤ Bench
- ⑥ Signage
- ⑦ Street Lighting
- ⑧ Trash Can
- ⑨ Street Parking



Long Beach Boulevard Specific Plan

Figure 5.3: Downtown Village I: Tweedy Boulevard Intersection Detail Plan



- 1 Mexican Fan Palm or Canary Island Date Palm
- 2 Ficus - Specimen Tree
- 3 Planting Area
- 4 Enhanced Paving
- 5 Monument
- 6 Pedestrian Walk
- 7 Median
- 8 Street Parking

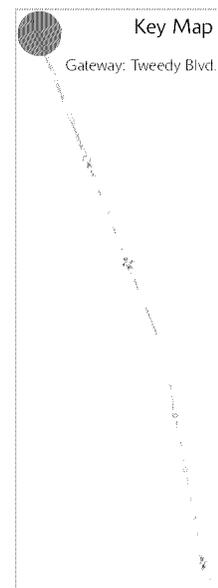
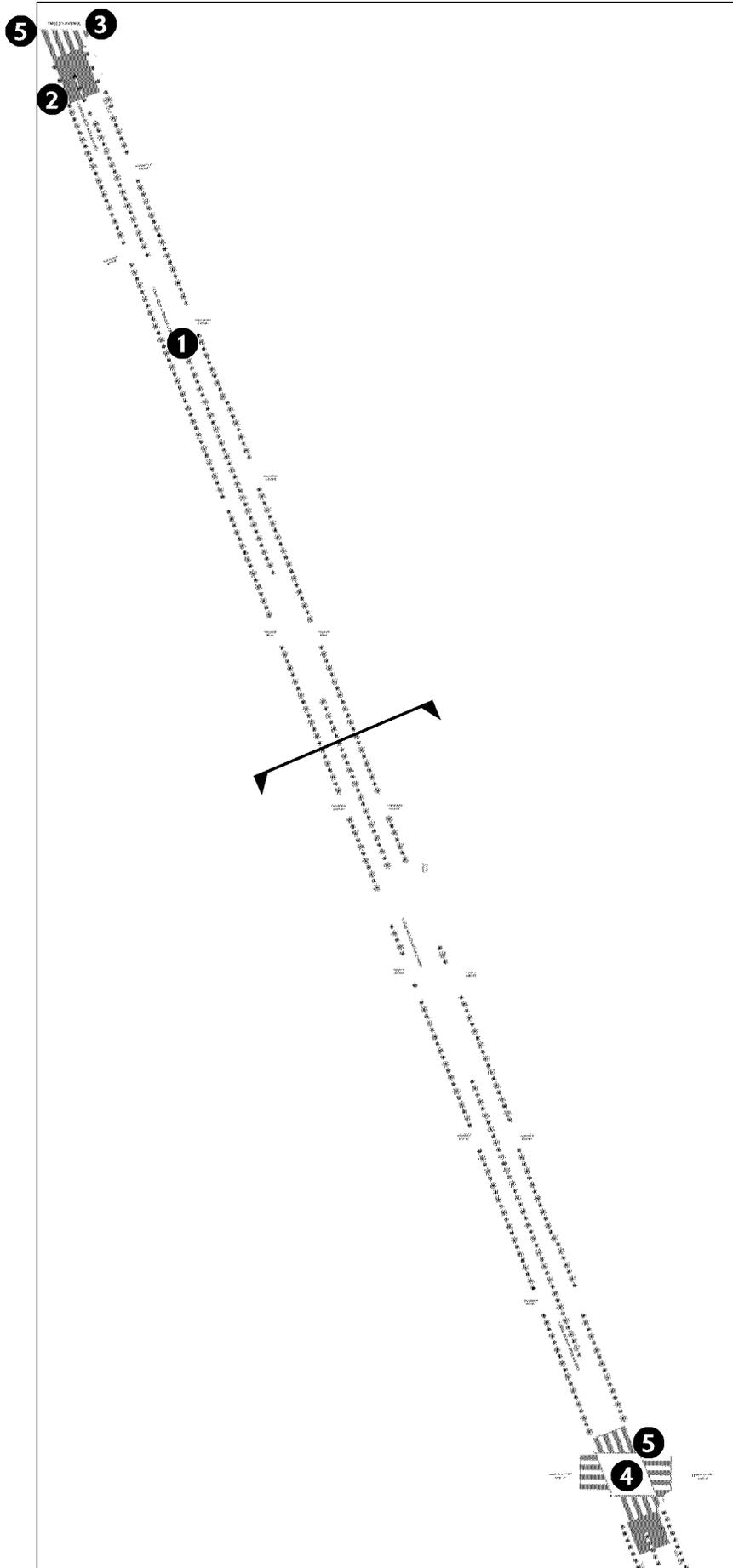
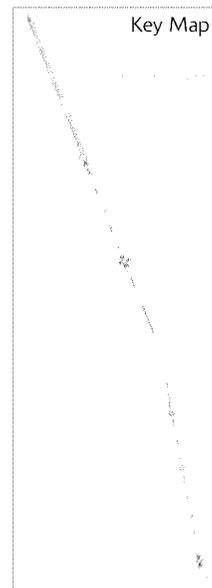


Figure 5.4: Downtown Village I: Street Tree Plan

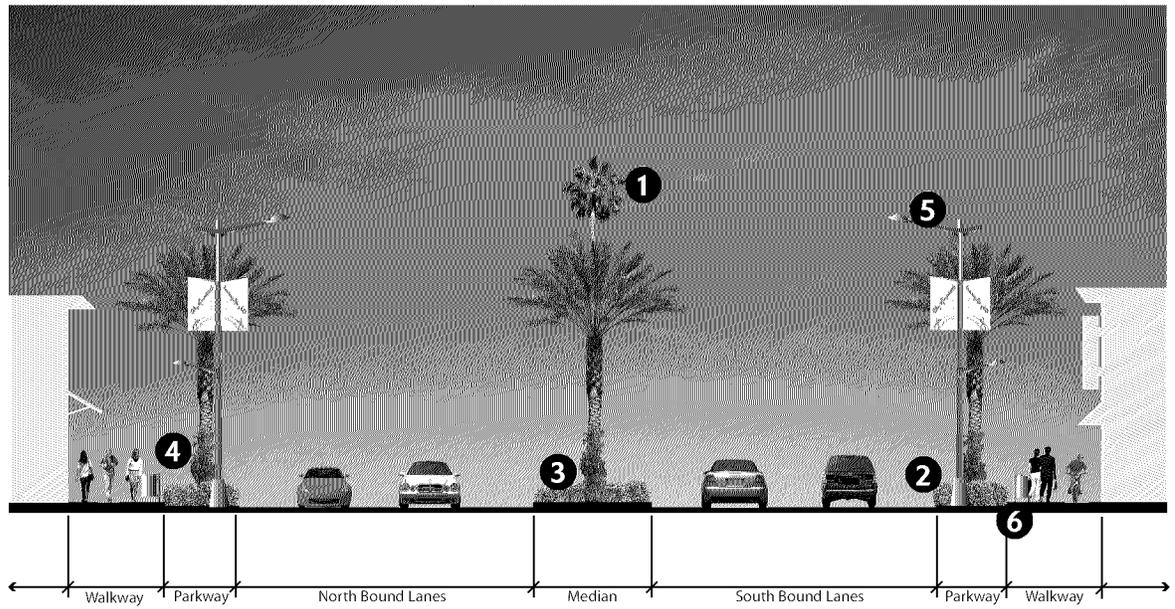


- 1 Mexican Fan Palm or Canary Island Date Palm
- 2 Ficus - Specimen Tree
- 3 Gateway at Tweedy
- 4 Intersection at MLK
- 5 Signage



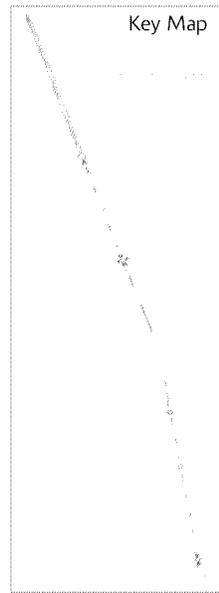
Long Beach Boulevard Specific Plan

Figure 5.5: Downtown Village I: Street Section



- ① Mexican Fan Palm or Canary Island Date Palm
- ② Shrub - Hedge
- ③ Daylily

- ④ Bougainvillea - Vine
- ⑤ Street Lighting
- ⑥ Trash Can



5.3.3 DOWNTOWN VILLAGE II DESIGN GUIDELINES

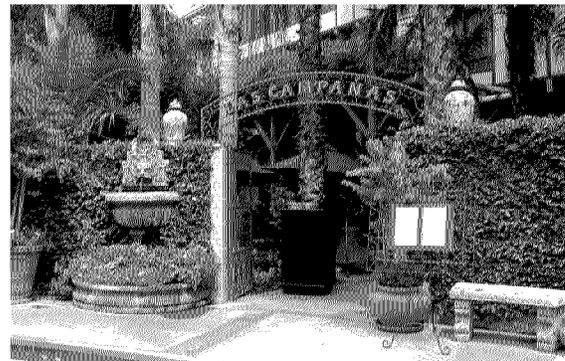
The following design guidelines apply to Village II:

- **Minimum sidewalk width:** 18 feet.
- **Planting width:** 3-4 feet.
- **Material:** PCC.
- **Planting Materials:** Mexican Fan Palm or Canary Island Date Palm, Ficus (specimen tree), and Bougainvillea. Figures 5.6 through 5.10 illustrate the landscape plan for Downtown Village II.
- **Street Furnishings:** Street lighting, outdoor seating, bicycle racks, trash receptacles, and other amenities are provided on the sidewalk.
- Street lighting will also carry banners designed to announce special civic or cultural events.
- Water elements should be implemented into the streetscape without acting as a barrier to pedestrian circulation. Small fountains or vertical water falls attached to the building façade would work best due to the limited sidewalk space.
- **Street Intersections:** The major intersection at Imperial Highway and Long Beach Boulevard is marked by enhanced paving across the intersection. The pedestrian walking zone is especially important as this is a busy intersection with movement of pedestrians, bicyclists, and vehicles.
- Pedestrian bulb-outs should be considered to reduce the pedestrian crossing distance and time. This also improves pedestrian visibility.



Intersection of Imperial Highway and Long Beach Boulevard.

- **Landmark Element:** A landmark element in the form of an archway or gateway is planned at the northeast and southwest corner of Imperial Highway and Long Beach Boulevard. This should be a major landmark that adds to the ambience of this intersection as a “Town Square” for the City.



Small ornate water fountain next to a business entrance.

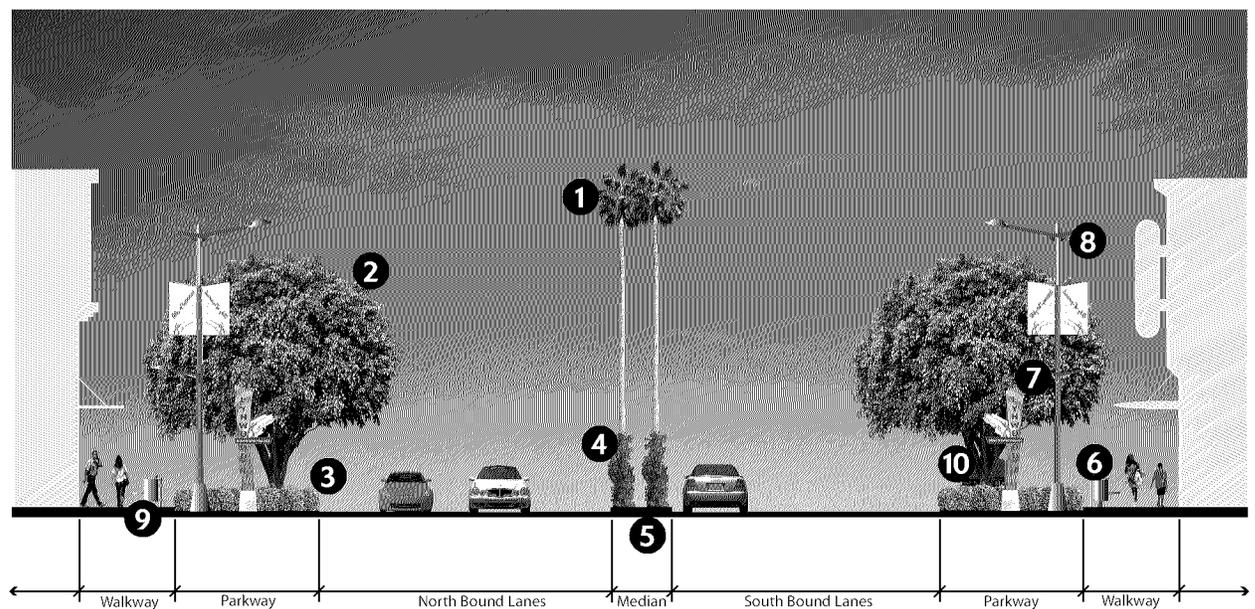
5.3.4 VILLAGE III-TRANSIT VILLAGE DESIGN GUIDELINES

The following design guidelines apply to Village III-Transit Village:

- **Minimum sidewalk width:** 18 feet.
- **Planting width:** 3-4 feet.

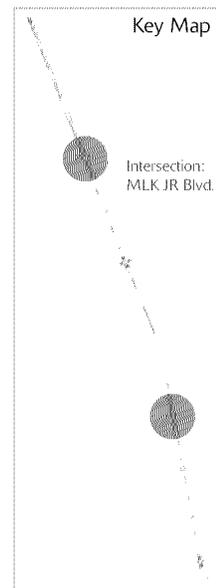
Long Beach Boulevard Specific Plan

Figure 5.6: Intersection at Martin Luther King Jr. Boulevard



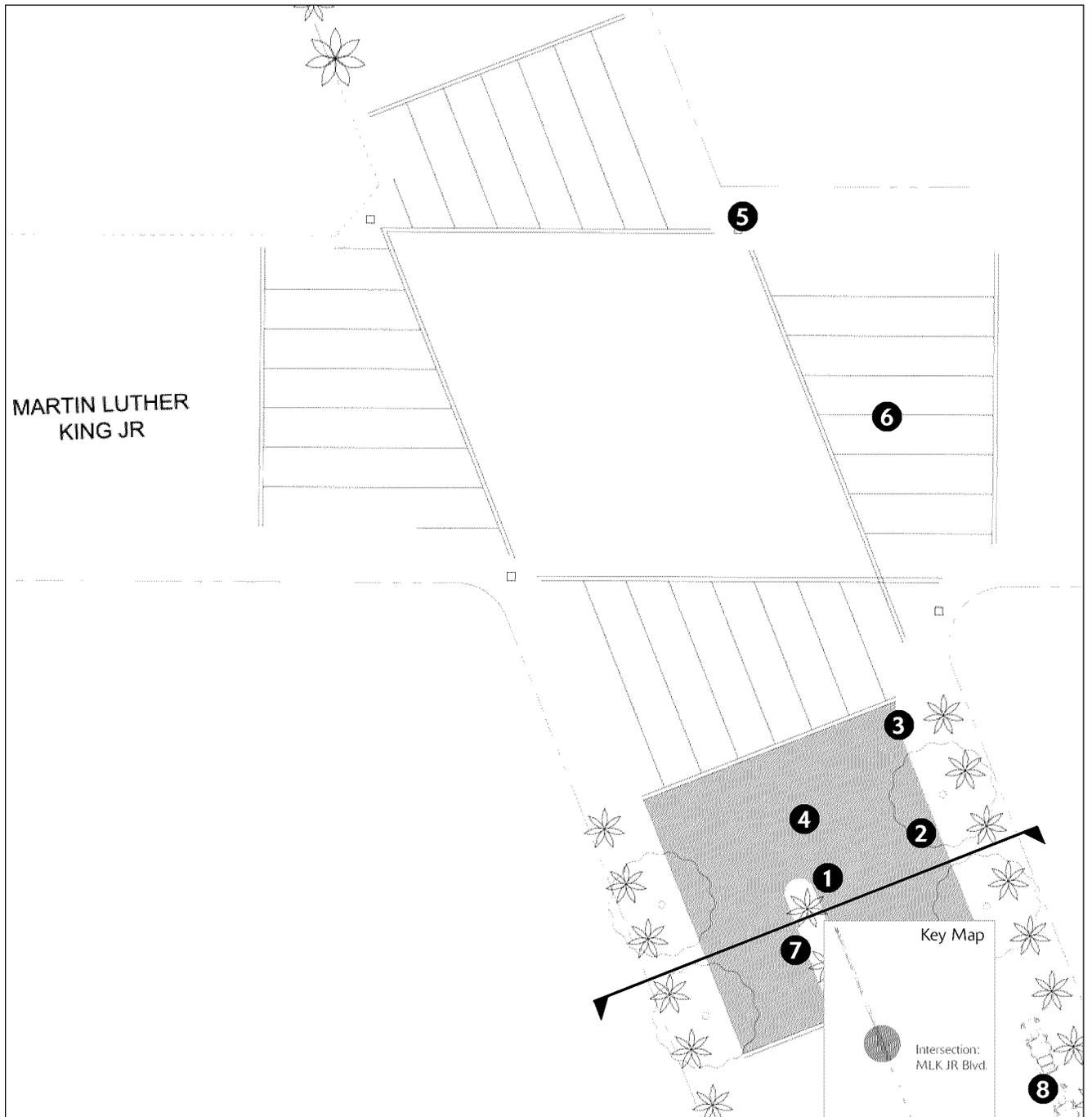
- 1 Mexican Fan Palm or Canary Island Date Palm
- 2 Ficus - Specimen Tree
- 3 Shrub - Hedge
- 4 Bougainvillea - Vine
- 5 Median

- 6 Bench
- 7 Signage
- 8 Street Lighting
- 9 Trash Can
- 10 Street Parking



Long Beach Boulevard Specific Plan

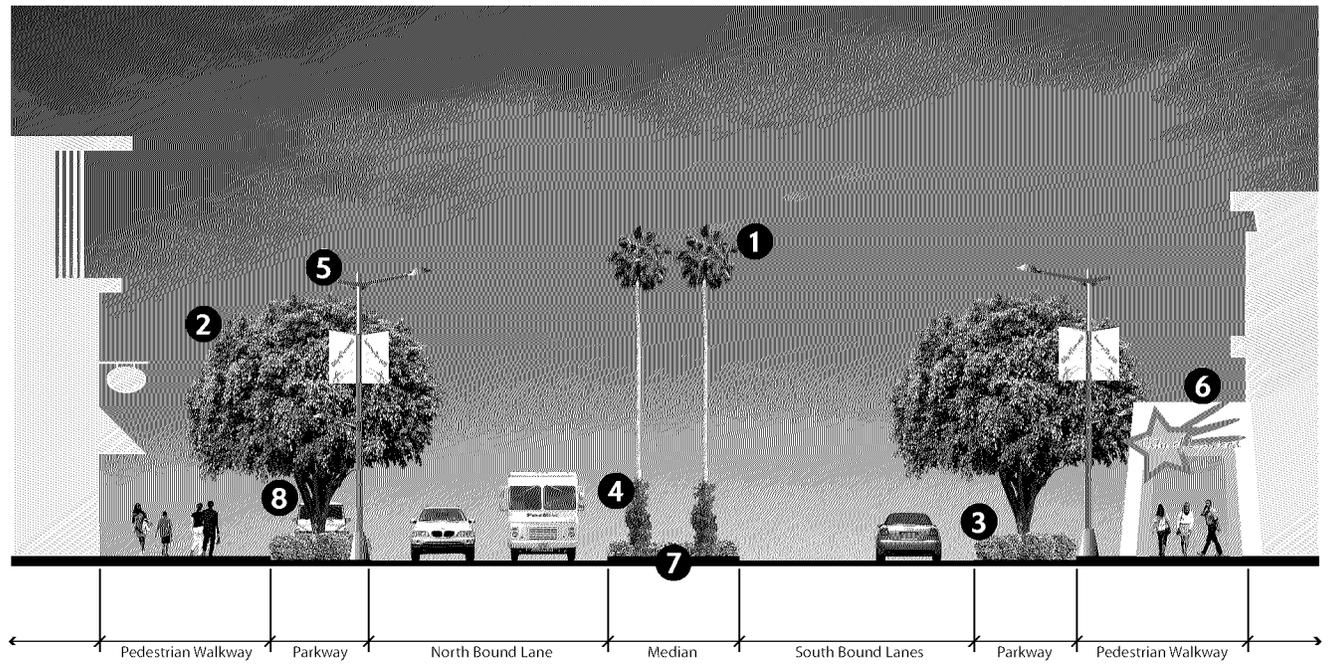
Figure 5.7: Downtown Village II: Martin Luther King Jr Boulevard Intersection Detail Plan



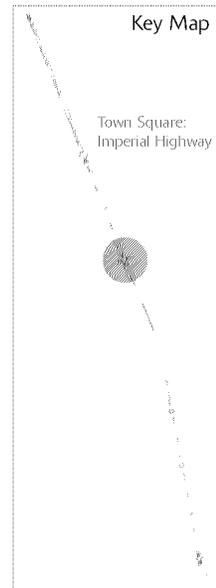
- ① Mexican Fan Palm
- ② Ficus - Specimen Tree
- ③ Planting Area
- ④ Enhanced Paving
- ⑤ Signage
- ⑥ Pedestrian Walk
- ⑦ Median
- ⑧ Street Parking

Long Beach Boulevard Specific Plan

Figure 5.8: Intersection at Imperial Highway

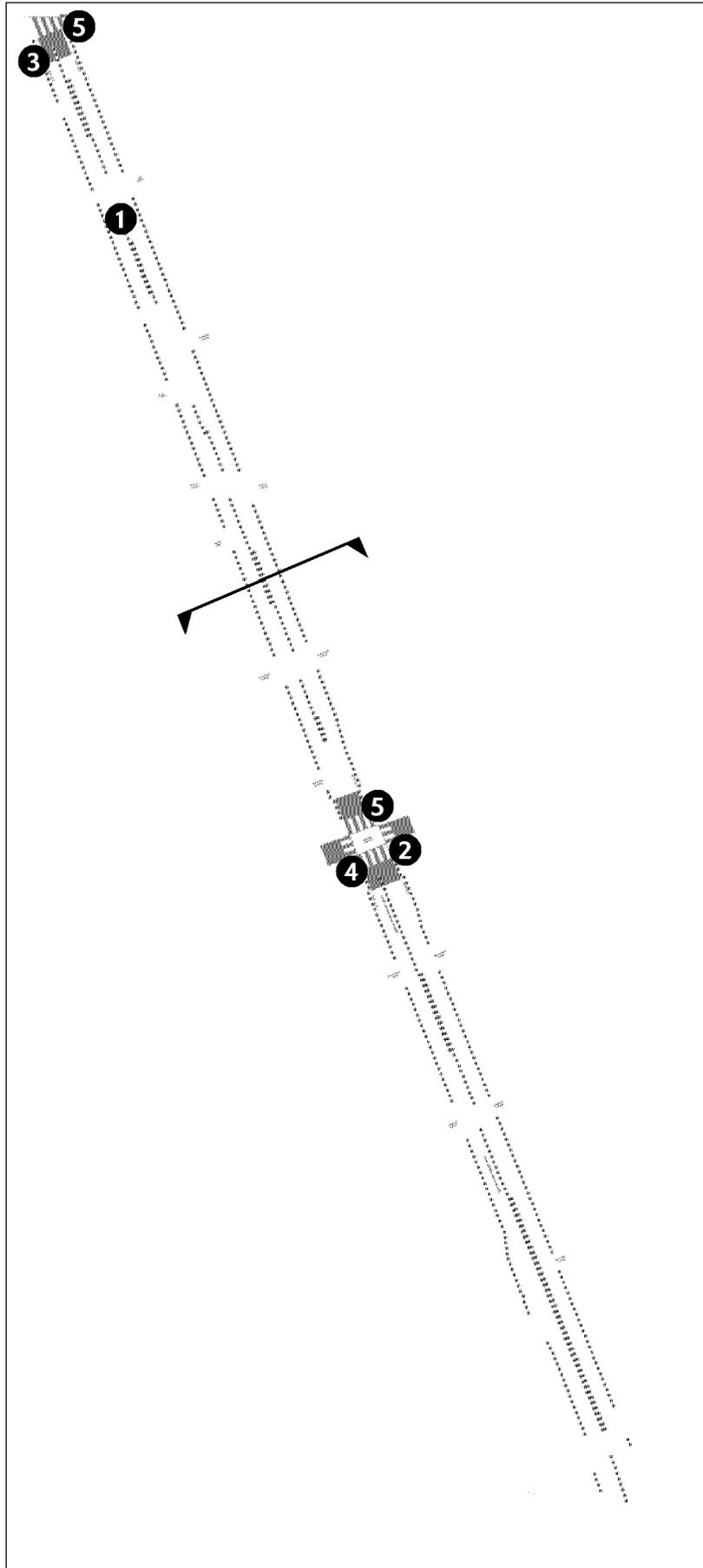


- 1 Mexican Fan Palm or Canary Island Date Palm
- 2 Ficus - Specimen Tree
- 3 Shrub - Hedge
- 4 Bougainvillea - Vine
- 5 Street Lighting
- 6 Signage
- 7 Median
- 8 Street Parking

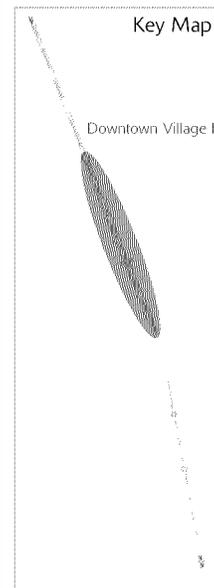


Long Beach Boulevard Specific Plan

Figure 5.9: Downtown Village II: Street Tree Plan

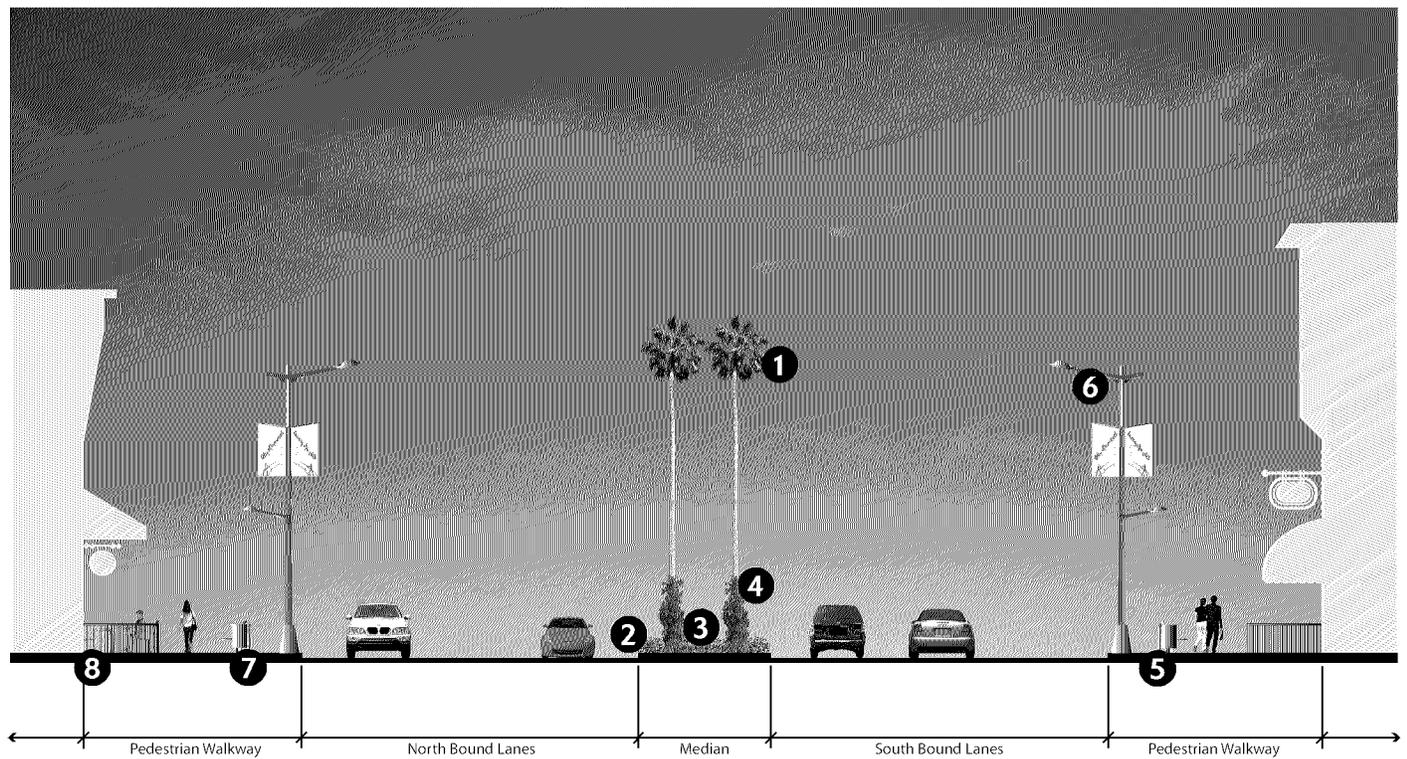


- 1 Mexican Fan Palm
- 2 Ficus - Specimen Tree
- 3 Intersection at MLK
- 4 Town Square at Imperial
- 5 Signage

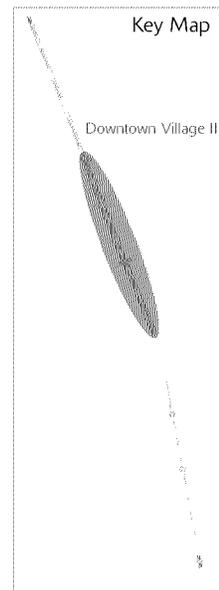


Long Beach Boulevard Specific Plan

Figure 5.10: Downtown Village: Street Section



- ❶ Mexican Fan Palm
- ❷ Shrub - Hedge
- ❸ Daylily
- ❹ Bougainvillea - Vine
- ❺ Bench
- ❻ Lighting
- ❼ Trash Can
- ❽ Patio



- **Material:** PCC.
- **Planting Materials:** Mexican Fan Palm, or Canary Island Date Palm, Ficus (specimen tree), and Bougainvillea. Landscape palette is shown in Figures 5.11 through 5.14.
- **Street Furnishings:** Street lighting, outdoor seating, bicycle racks, trash receptacles, and other amenities are provided on the sidewalk. Bicycle racks, security, and lighting should be enhanced under I-105 along the Boulevard.
- Street lighting will also carry banners designed to announce special civic or cultural events.
- Water elements should be implemented into the streetscape without acting as a barrier to pedestrian circulation. Small fountains or vertical water falls attached to the building façade would work best due to the limited sidewalk space.
- **Street Intersections:** Josephine Street and Long Beach Boulevard is marked by enhanced paving and crosswalks for safe pedestrian crossings. Embedded flashing lights for the crosswalks at Interstate 105 and Long Beach Boulevard are recommended.
- **Landmark Element:** A landmark element in the form of an archway or gateway is planned at the intersection of Josephine Street and Long Beach Boulevard. Way finding signs should be planned to show the different components of Green Line.



Traveling past Josephine Street toward the Green Line station currently lacks pedestrian crosswalks and appropriate landmarks.



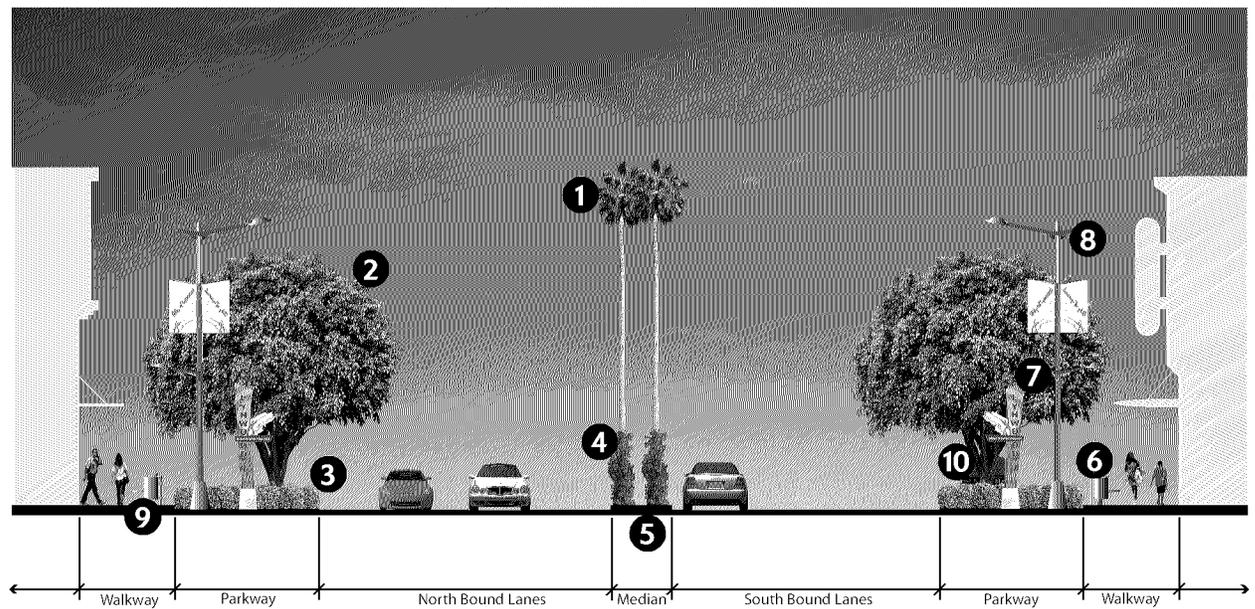
This part of the median is divided with a fence to prevent pedestrians crossing the street. The pedestrian-scaled light post seems out of place since pedestrians are deterred from using the space.



Land uses currently in the Transit Village cater to vehicles such as gas stations and U-Haul rental lots. The conceptual TOD plan calls for pedestrian-friendly uses instead, such as cafés and bookstores.

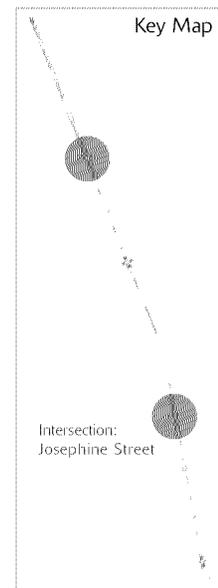
Long Beach Boulevard Specific Plan

Figure 5.11: Intersection at Josephine Street



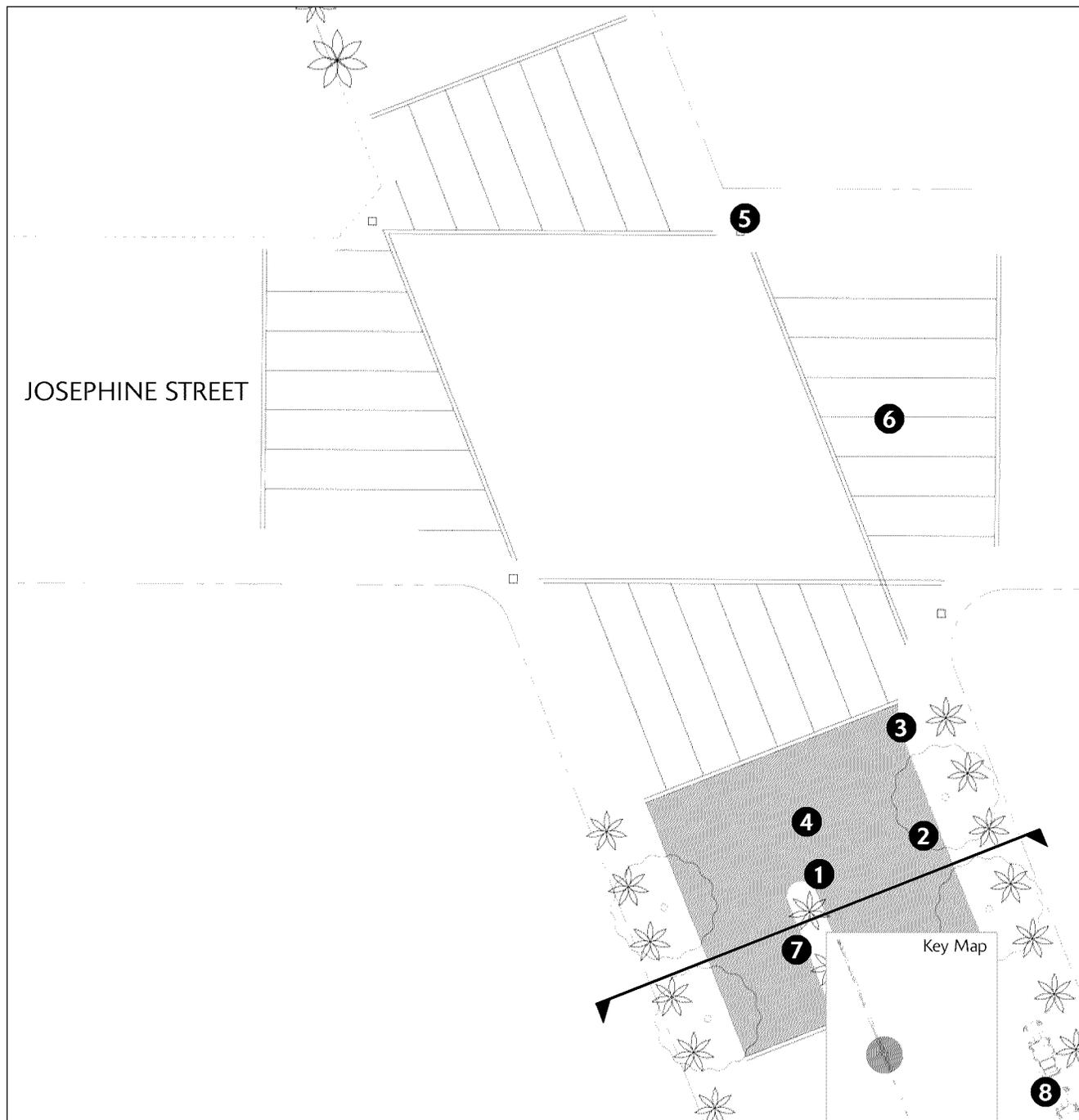
- 1 Mexican Fan Palm or Canary Island Date Palm
- 2 Ficus - Specimen Tree
- 3 Shrub - Hedge
- 4 Bougainvillea - Vine
- 5 Median

- 6 Bench
- 7 Signage
- 8 Street Lighting
- 9 Trash Can
- 10 Street Parking



Long Beach Boulevard Specific Plan

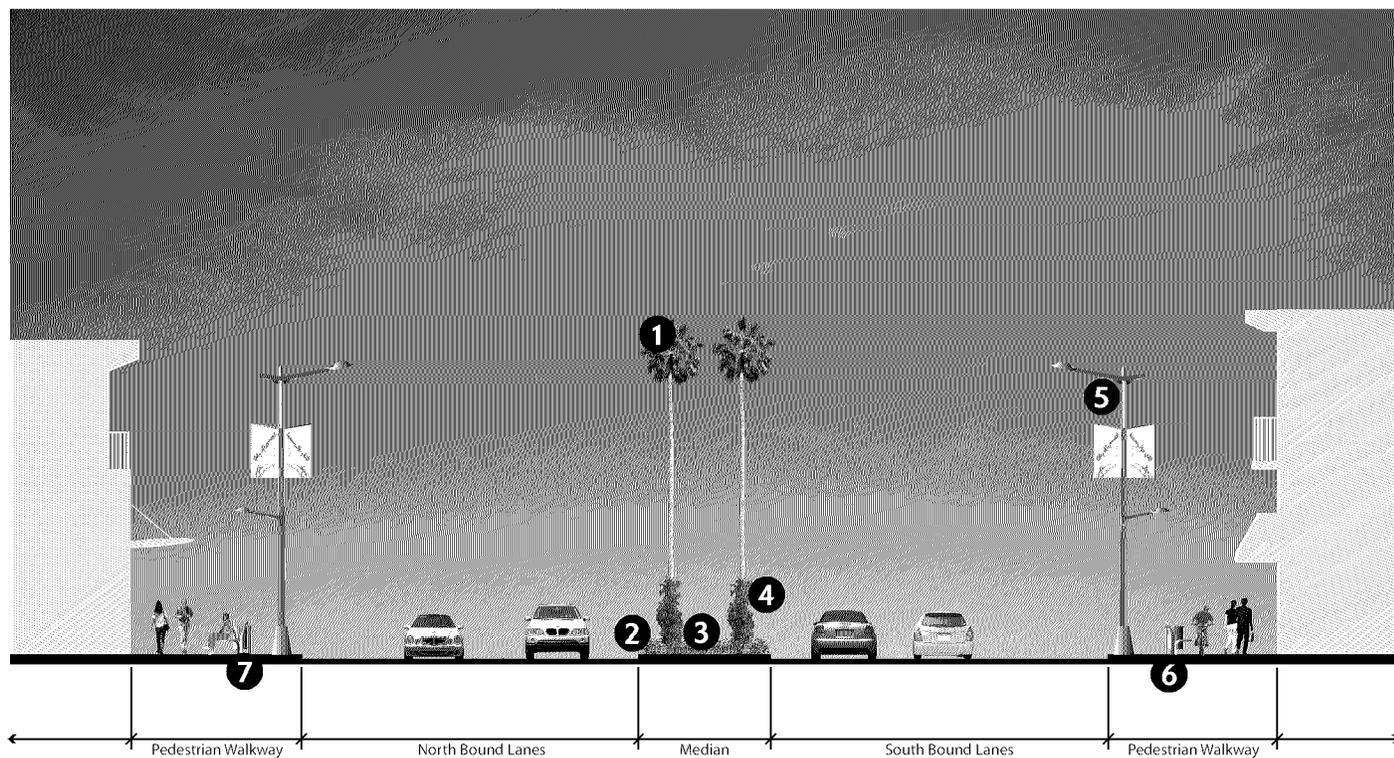
Figure 5.12: Transit Village: Joeephine Street Intersection Detail Plan



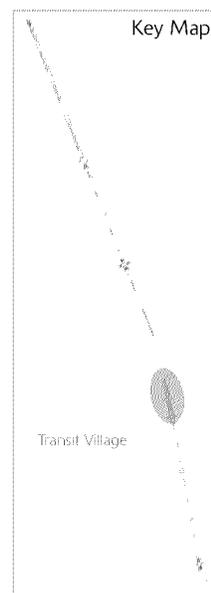
- 1 Mexican Fan Palm
- 2 Ficus - Specimen Tree
- 3 Planting Area
- 4 Enhanced Paving
- 5 Signage
- 6 Pedestrian Walk
- 7 Median
- 8 Street Parking

Long Beach Boulevard Specific Plan

Figure 5.14: Transit Village: Street Section



- 1 Mexican Fan Palm
- 2 Shrub - Hedge
- 3 Daylily
- 4 Bougainvillea - Vine
- 5 Lighting
- 6 Bench
- 7 Trash Can



5.3.5 VILLAGE IV – BUSINESS VILLAGE DESIGN GUIDELINES

The following design guidelines apply to Village IV:

- **Minimum sidewalk width:** 18 feet.
- **Planting width:** 3-4 feet.
- **Material:** PCC.
- **Planting Materials:** Mexican Fan Palm, Camphor Tree, Daylily, Bougainvillea, and Shrub (Hedge). Landscape palette placement is located in Figures 5.15 through 5.17.
- **Street Furnishings:** Street lighting, outdoor seating, bicycle racks, trash receptacles, and other amenities are provided on the sidewalk. Bicycle racks, security, and lighting should be enhanced under I-105 along the Boulevard.
- Street lighting will also carry banners designed to announce special civic or cultural events.
- Water elements should be implemented into the streetscape without acting as a barrier to pedestrian circulation. Small fountains or vertical water falls attached to the building façade would work best due to the limited sidewalk space.
- **Street Intersections:** Euclid Avenue is completely contained within the City limits and is marked by enhanced paving and crosswalks for safe pedestrian crossings.
- **Landmark Element:** A landmark element in the form of an archway or gateway is planned at the intersection of Euclid Avenue and Long Beach Boulevard.



Most businesses, such as this restaurant located just south of Cedar Avenue, offer parking on the side of the building.

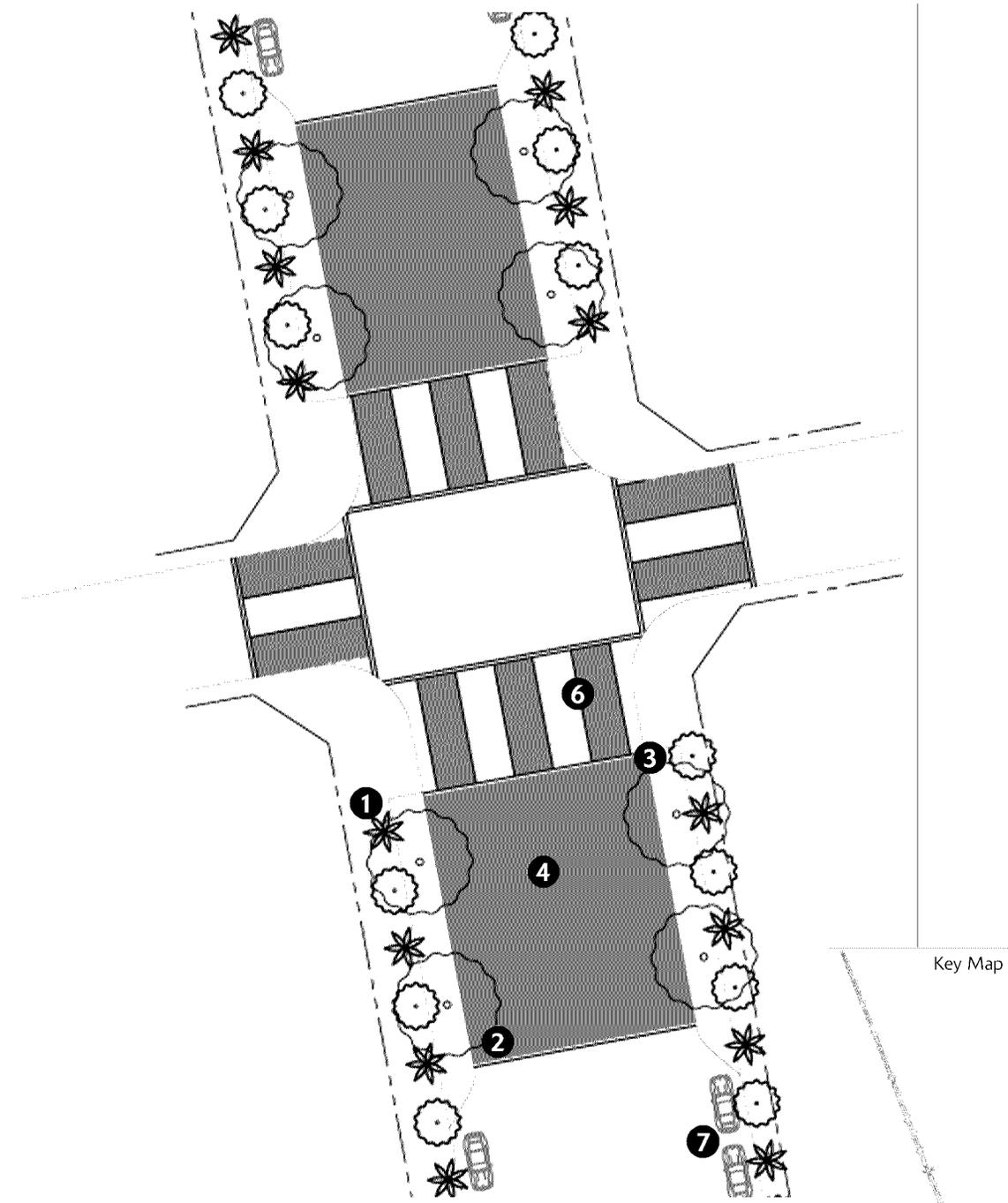


The intersection of Burton Avenue and Long Beach Boulevard experiences a high level of traffic.



The median in front of these one-story businesses between Euclid and Orchard Avenues helps mitigate traffic while adding landscape elements.

Figure 5.15: Euclid Intersection Detail Plan

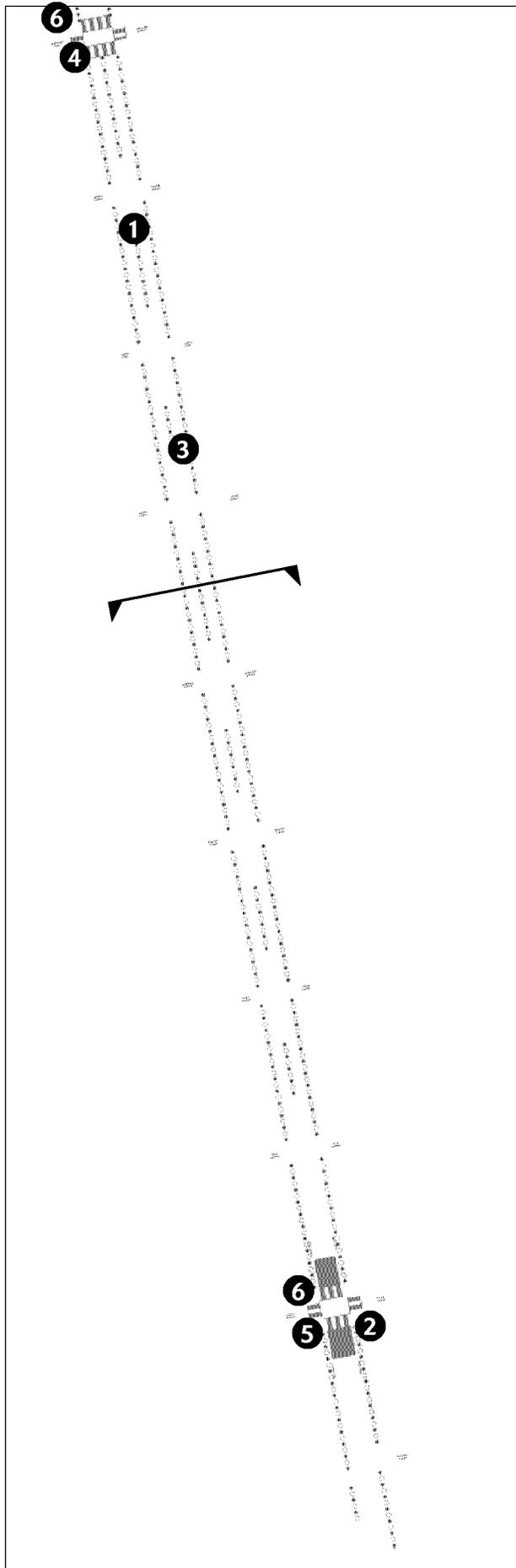


- | | |
|-------------------------|-------------------|
| ① Mexican Fan Palm | ⑤ Signage |
| ② Ficus - Specimen Tree | ⑥ Pedestrian Walk |
| ③ Planting Area | ⑦ Street Parking |
| ④ Enhanced Paving | |

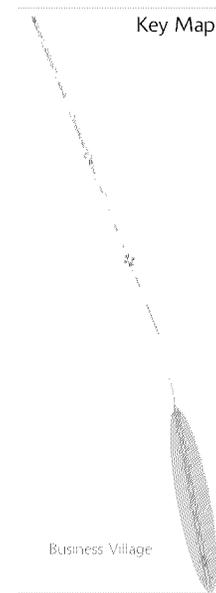
Intersection:
Euclid Avenue

Long Beach Boulevard Specific Plan

Figure 5.16: Business Village: Street Tree Plan

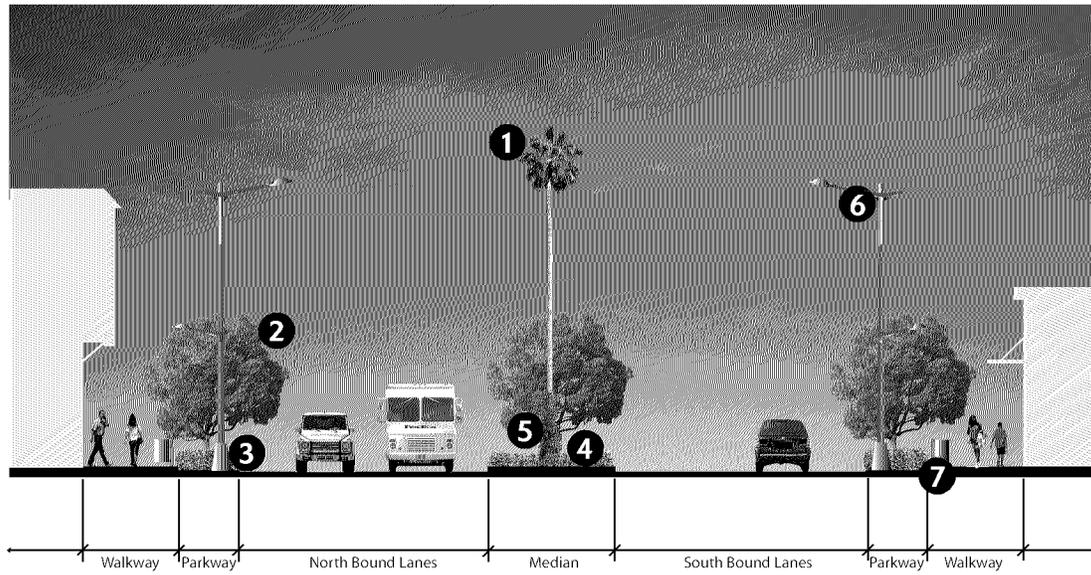


- 1 Mexican Fan Palm
- 2 Ficus - Specimen Tree
- 3 Camphor Tree
- 4 Intersection at Josephine
- 5 Gateway at Euclid
- 6 Signage

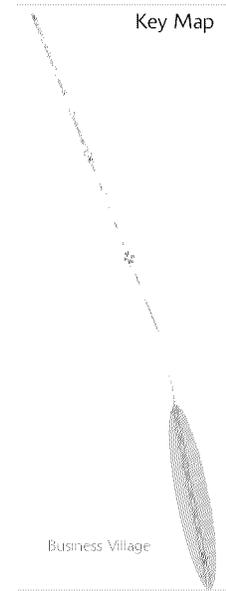


Long Beach Boulevard Specific Plan

Figure 5.17: Business Village: Street Section



- 1 Mexican Fan Palm
- 2 Camphor Tree
- 3 Shrub - Hedge
- 4 Daylily
- 5 Bougainvillea - Vine
- 6 Street Lighting
- 7 Trash Can



Nodes and Intersections. Based on the land use organization of four villages along the Boulevard, the landscape plan reinforces and echoes the transitions between villages by the design of intersections or major nodes. The layout approach to these intersections is marked by special stamped concrete pavers and labeled enhanced paving at Tweedy Boulevard, Imperial Highway, Martin Luther King Jr. Boulevard, Josephine Street, and Euclid Avenue as seen in previously referenced Figures 5.2 through 5.17.



The crosswalk is emphasized with scored/stamped concrete or other paving materials.

Signage Entry Monuments. The entrance to the City from I-105 serves as the gateway to the City. Major traffic flows in and out of the City along Long Beach Boulevard. It is recommended that the City implement major landmark entry monuments at the intersection of Imperial Highway and Long Beach Boulevard. This intersection functions as the “Town Square.”

Other locations for gateway/archway monuments are Tweedy Boulevard, Martin Luther King Jr. Boulevard, Josephine Street, and Euclid Avenue. The archway as an entry monument is described in the next section.

5.3.6 GATEWAYS AND ARCHWAYS AS SIGN MONUMENTS

Successful neighborhoods, villages and downtown areas often implement community elements such as uniform signage, gateways or archways, and custom logos that represent the culture and values of the local population.

Beautifully created archways are seen in many communities across America as a street element to revitalization. Some of the examples include “El Archo” on Whittier Boulevard in Los Angeles; the Gaslamp District in San Diego; and Downtown and Old Sacramento in Sacramento.

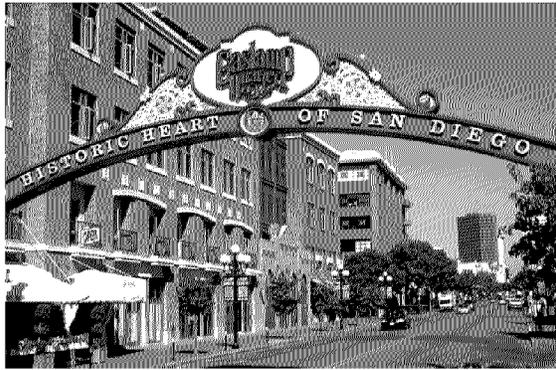
Appropriately designed gateways, archways and bridges can help reinforce identity and a sense of pride in Lynwood. The circulation and landscape plan envisions five archways to be placed at the following intersections along Long Beach Boulevard:

- Tweedy Boulevard;
- Martin Luther King Jr. Boulevard;
- Imperial Highway;
- Josephine Street; and
- Euclid Avenue.

These distinctive gateways/archways mark the entry and transitions to the four villages described in the Specific Plan.

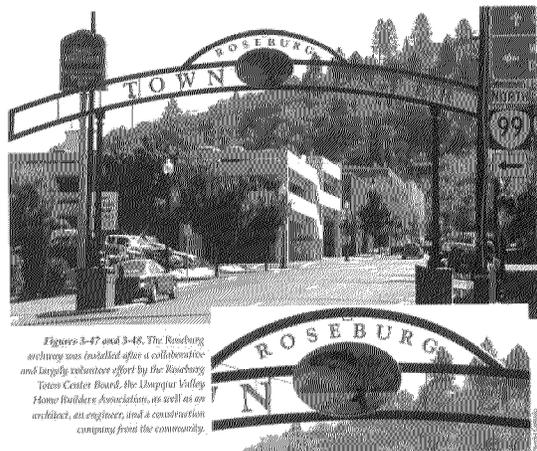
The gateway design may consist of sculpted signs, pylons, flags and banners, landscaping elements, gate and arches, or a combination of these elements. Some examples are illustrated to provide guidance. Local community participation should guide the design development process of archways or gateways. Design

of the gateway should incorporate elements of Spanish Colonial and Italianate styles.



The Gaslamp District in San Diego, California.
Source: www.sandiegodowntown.org

The example shown above of the popular Gaslamp District of San Diego includes an archway that complements the building façades. It is lit at night and creates a festive entertainment ambiance.



Figures 3-47 and 3-48. The Roseburg archway was installed after a collaborative and largely volunteer effort by the Roseburg Town Center Board, the Douglas Valley Home Builders Association, as well as an architect, an engineer, and a construction company from the community.

In 1999, the community of Roseburg, Oregon, adopted a downtown master plan for its Town Center, which called out for two gateway arches signifying arrival entry points into downtown. Source: "Placemaking on a Budget: Improving Small Towns, Neighborhoods, and Downtowns Without Spending a Lot of Money," 2005.

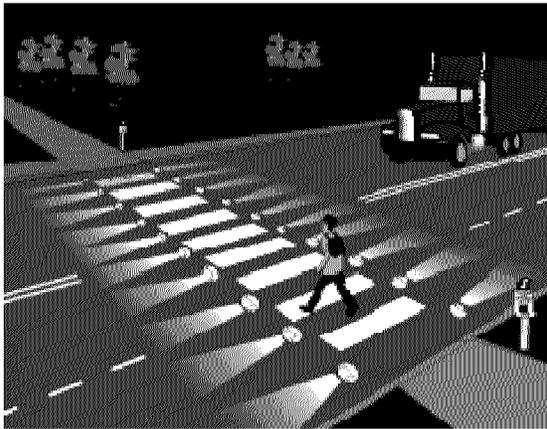
5.3.7 PEDESTRIAN CROSSWALKS AND SAFETY

Pedestrian crosswalks are a critical part of the circulation plan and are in extreme need of improvements throughout Long Beach Boulevard. The off-ramp of I-105 at Long Beach Boulevard presents a safety hazard for people who walk and bicycle to the parking lot under I-105 for the Green Line station.



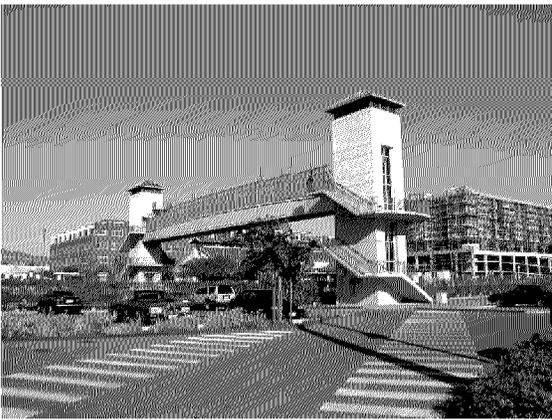
Existing Off-ramp at I-105

All crosswalks should provide for a more comfortable and safe pedestrian environment. The pedestrian crosswalk should be upgraded with high visibility markers for vehicles exiting and entering the freeway. Further safety enhancements such as bollards, well lit wayfinding signs, and warning signs and lights (such as flash lights) will help to improve visibility, particularly at night. The main function of bollards is to prevent traffic from encroaching into pedestrian areas while also contributing to the overall streetscape with attractive colors and shapes. Wayfinding signs should be oriented to both the drivers and pedestrians, providing direction for a short distance to lower speeds.



Mounted warning/flashing lights.

These crosswalk elements are necessary for the grade-level crossing to the transit area as well as other crosswalks indicated in previously referenced Figure 3.2-Pedestrian Pathways and Crosswalks. In the example of the Emeryville Station, a pedestrian bridge allows for pedestrians to enter and exit the transit station in a safe manner while also acting as an entry gateway to the City.



Emeryville Station Pedestrian Bridge. Source: www.dot.ca.gov

In some instances of longer blocks, incorporating curb extensions for midblock crossings helps pedestrians. Mid-block crossings are pedestrian crosswalks that are not at intersections. They help to decrease the crossing times and increase the visibility between pedestrians and drivers.



Embedded warning/flashing lights with lit bollards at a midblock crossing.

Each existing and proposed crosswalk shall be consistent with appropriate materials, colors, scale, and elements. The street width and traffic level will help determine what elements shall be appropriate for each individual crosswalk.

Improved crosswalks leading to pedestrian pathways and connecting to courtyards and/or plazas ensure a successful continuous connection with increased foot traffic for the local businesses as well as movement to the transit station.

5.3.8 BICYCLE PATHS AND PARKING

City of Lynwood has all the ingredients necessary to become a great “bicycle-friendly” community. The Metro Bus and the Green Line allow bicycles onboard without permits. Existing issues such as air quality, traffic congestion, lack of parking, and quality of life help foster the exploration of innovative approaches to building a city that will accommodate new growth in a positive, creative way while protecting basic community values. The goal is to create a strong bicycle/pedestrian program that will enable the residents of Lynwood to use their bicycles in a safe and efficient manner.

By using proper bike lanes, trails, and secondary streets, bicyclists will be able to move about the town quickly and safely. The relatively flat (1% or less) topography and the generally mild climate encourages year-round riding. Often, bicycles are the fastest way to commute five miles or less. On average, a 10-minute walk can cover $\frac{1}{4}$ of a mile, whereas a 10-minute bike ride can cover about 2 miles. The overall goal of this Plan is to make recommendations that can help create a physical environment free of barriers for those who choose to bicycle.

Arterial streets such as Long Beach Boulevard provide the most direct access to activity centers and other destinations. All arterial streets should be improved, over time, to become "bicycle friendly" and comprise the following characteristics:

- On-street lanes with a lane width of 4 feet to 6 feet between the bicycle lane stripe and the joint between the pavement and the gutter pan.
- May include short off-street sections to bypass major bottlenecks.
- Signalized intersections or through/priority travel (side streets stop).
- High-visibility signage and lane marking.
- High priority maintenance of bicycle travel areas.

The project area requires improvements for bicycle transportation that provide direct and convenient access to major activity centers and other destinations such as the Green Line, employment centers, downtown, schools, shopping, government buildings, and parks and other recreational destinations. Special

attention should be given to access the following:

- Destinations along and across Long Beach Boulevard (especially at Martin Luther King Jr. Boulevard, Imperial Highway, etc.);
- Neighborhood shopping centers;
- Plaza Mexico;
- Employers on the northern and southern blocks of the project area;
- The Transit Green Line Station; and
- Along and across I-105.

Potential improvements for bicyclist should include bicycle lane striping. Bicycle lane stripes sometimes end before intersections, leaving the bicyclist without clear direction about where to ride. Usually, the stripes are discontinued because the right-of-way is not of sufficient width to stripe bike lanes and provide for other intersection improvements, such as right-turn or left-turn bays. At intersection approaches, the bike lane striping should be dashed to indicate that motorists may enter or cross the bike lane to make a right turn. Use of other pavement markings and signs is also recommended under such conditions. Where there is not adequate width to stripe the bike lane up to the intersection approach, the curbside lane should be signed as a shared-use lane. In some cases, use of the shared-use lane is restricted to buses, bicycles, and right turns. Some bike lanes are striped using paint, which typically lasts only two years. Some bike lanes are striped using thermoplastic, which can last three to five years.

Bicycle Parking. The two factors most important to bicyclists wishing to park their bikes are security and proximity to

destinations. A third important factor is availability of bike racks that are designed to allow use of high-security locks and locking methods.

Class I bicycle parking consists of bicycle lockers, restricted access facilities such as a locked room or garage, or an enclosed cage. Class II bicycle parking consists of on-street bicycle racks.

Convenient and secure bicycle parking facilities shall be provided within each future development. Additionally, the City should provide "U" bicycle racks on side streets within each village on Long Beach Boulevard.

The following bicycle parking requirements shall apply:

- **Non-residential.** Minimum 5 percent of the total number of automobile parking spaces provided.
 - **Office:** 80 percent of bicycle spaces should be Class I and 20 percent Class II.
 - **Retail:** 20 percent of bicycle spaces should be Class I and 80 percent Class II.
- **Residential.** Minimum of one Class I bicycle parking space per unit.

Bicycle Racks. Bicyclists, like motorists, look for convenient and secure parking places near their destinations. Often, bicyclists have few choices and will simply lock bicycles to nearby stationary objects, such as signs, railings, and parking meters on busy downtown sidewalks. Such an unstructured approach to bicycle parking inconveniences pedestrians and bicyclists alike and promotes the feeling that bicycles are unwelcome. Well-designed, attractive and well-placed racks make it obvious that

bicycles are an important part of the transportation system.

The inverted "U" bicycle rack is recommended by the Association of Pedestrian and Bicycle Professionals. Simple, functional, and durable, the inverted "U" rack allows the user to lock two bicycles with either a standard U-shaped lock or a chain/cable and lock. The racks have no edges, seams, or hardware to pose a hazard or become unsightly.

5.4 STREET FURNISHINGS

Street furniture typically includes amenities located on sidewalks, and within plazas or courtyards. Elements of street furniture include benches, tables, chairs, trash receptacles, game tables, banners, bicycle racks, lighting, water elements, potted planters, kiosks, transit/bus shelters, signage, and other elements.

5.4.1 LANDSCAPE PLANTERS

Potted plants are encouraged to integrate plants into the street landscape with more flexibility for placement and relocation.

Placement of planters should be in the furnishing zone as described earlier in this chapter, as well as courtyards or places where tree plantings are not allowed. Landscape planters on sidewalks shall be spacious and include at least one mature tree. These planters should be alternated on each side of the street for a checkerboard effect.

Preferably, the style of planters should reflect the architectural theme of the village of its location. It should include concealed lighting fixture to provide a downward pool of light to the sidewalk.

Window planter boxes are encouraged for smaller scale buildings to create visual interest and to soften the appearance within existing developments lacking sufficient space for streetscape landscaping.

5.4.2 BUS SHELTERS

Bus stop locations should include a shelter for waiting passengers. Minimum size of bus shelters should be 6 feet wide and between 10 and 25 feet long. Basic amenities shall include a seating bench, trash receptacle, lighting, signage, and, where possible, a lighted enclosure to display the bus system map and schedules. Shelters may include electronic display signs to show actual arrival time of buses. Bus stop signs must be at least 7 feet off the ground, and include route name, number, bus stop name, and transit system logo.



Bus shelters should be adjacent to the street and integrate into the streetscape design.

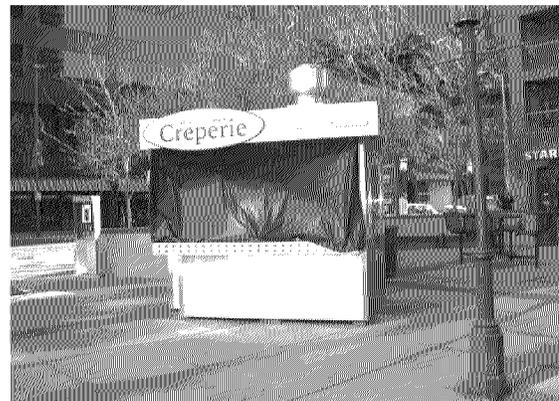
5.4.3 KIOSKS

Kiosks can range in size and purpose depending on location and sidewalk width. Preferred locations are well-lit and within plazas, shopping centers, wide sidewalk areas, and intersections with sidewalk bulb-outs. Kiosks may provide a

central location for community information, announcements, and maps. Some kiosks may include amenities such as newspapers racks, telephones, and tourist brochures. Popular commercial/retail kiosks typically sell flowers/gift baskets, sunglasses, cosmetics, hats, key chains, children's books, sports jerseys, purses, and watches. Other items may be food and drinks, Internet service, or an interactive reservations system to reserve movie tickets or dinner reservations within the area. City-owned kiosks may offer a place to pay utility bills, act as a substation for police services, postal services, or political events. Materials and paint should be vandal-resistant.



This informational kiosk has a representative available for questions and directions. The bold colors help to locate it from a distance.



Kiosks may have limited operating hours, such as this breakfast and lunch serving booth.

5.4.4 BENCHES, TRASH RECEPTACLES, TREE GUARDS AND OTHERS

Benches should be designed with backrests and be able to accommodate two to three people comfortably. Design style should include solid colors, clean lines, be fixed in place. Some benches may be set back to back as seen in Figure 5.18. Placement of benches is parallel to the sidewalk, within the edge zone and should face the building façade. Bench locations should be coordinated with transit services to be compatible with stops and waiting areas.

Trash receptacles should be placed near benches, building entrances, and within the edge zone along the sidewalk. Design style could include a metal round container, elevated off the ground, with an ashtray feature.



Chess or checker tables promote healthy social interactions.

Tree guards are permanent solutions to accommodate the development of a new tree into its full mature size without hindering the growth of the tree. Tree grates can be used as well and offer the flexibility of removal should the tree outgrow its space. Tree grates should be stable enough for pedestrians to walk on, be made out of metal, and correlate with

the design style of the benches and trash receptacles.

5.4.5 STREET LIGHTING

Street lighting should be located in the curb zone with two light fixtures as seen in Figure 5.18. Light fixtures should include an option for brackets to attach banners and other temporary graphics elements between the pedestrian fixture and street-level fixture. Light sources should include metal halide, fluorescent, or compact fluorescent luminaries.

Increasingly, jurisdictions are installing traffic poles that are custom designed with elements such as special signage and logo.

5.4.6 PUBLIC ART

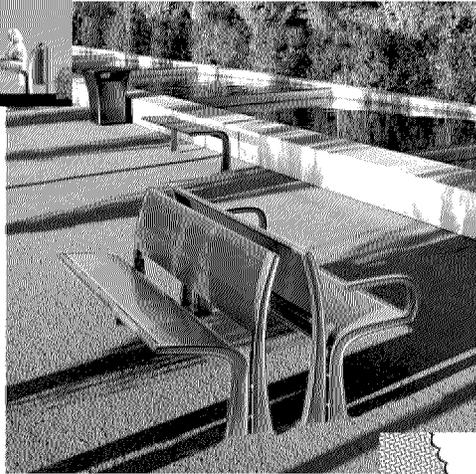
Public art offers an opportunity to commemorate a person, place, event, or time frame. Indirectly, public art helps to heighten a sense of community and pride. Public art can be utilized to help camouflage public infrastructure and utilities while enhancing the streetscape scene or can be integrated with seating and tables. Other ways to integrate public art into an urban environment include festive banners, murals on public and private buildings, sidewalk art (chalk drawings or tile designs), and stained glass art projects through a series of interrelated pieces constructed at several key locations within the median and public streetscape. The art pieces should provide colorful graphic depictions representative of the City's rich agricultural, cultural, and industrial history, and provide a thematic identity to the area. Art pieces can help define the village entries along Long Beach Boulevard, as well as at key nodes located near the intersections of Martin

Long Beach Boulevard Specific Plan

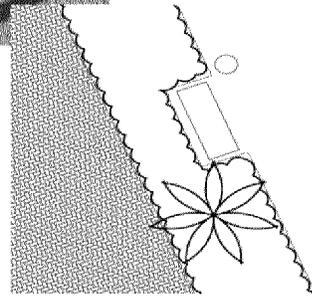
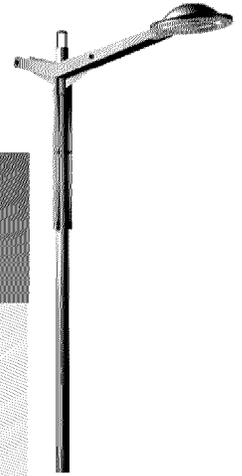
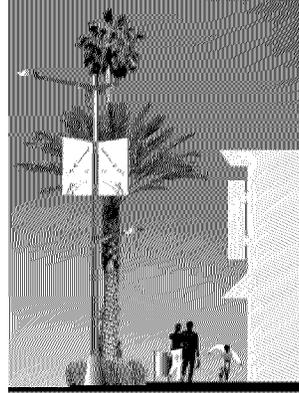
Figure 5.18: Street Furnishings



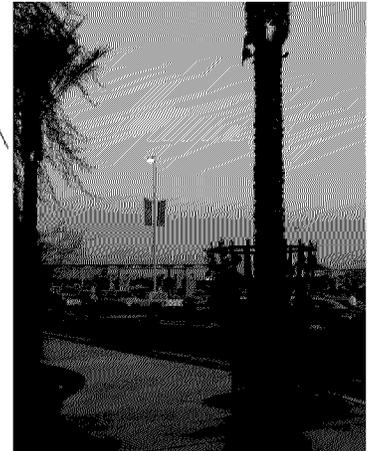
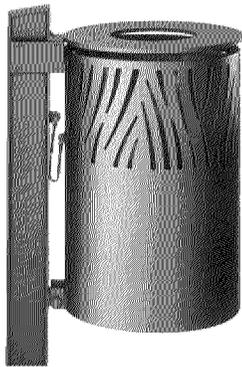
Bench



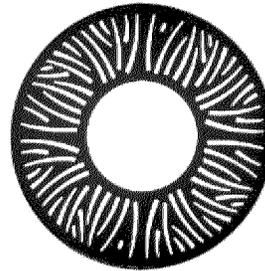
Lighting



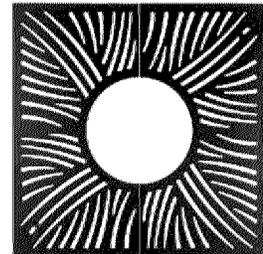
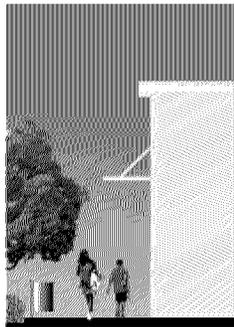
Trashcan Ashtray



Tree guard



Tree grate



Luther King Jr. Boulevard and Imperial Highway.



Public art can be easily integrated with seating in a courtyard or plaza setting.

Other street furnishings may include pedestrian-scaled clock posts, and water elements such as fountains.



Large public spaces can accommodate large-scale art pieces such as this face sculpture.

5.4.7 SIGNAGE

There are three types of signage considerations in an urban environment. These are:

- Street names;
- Way finding signs; and
- Business signs

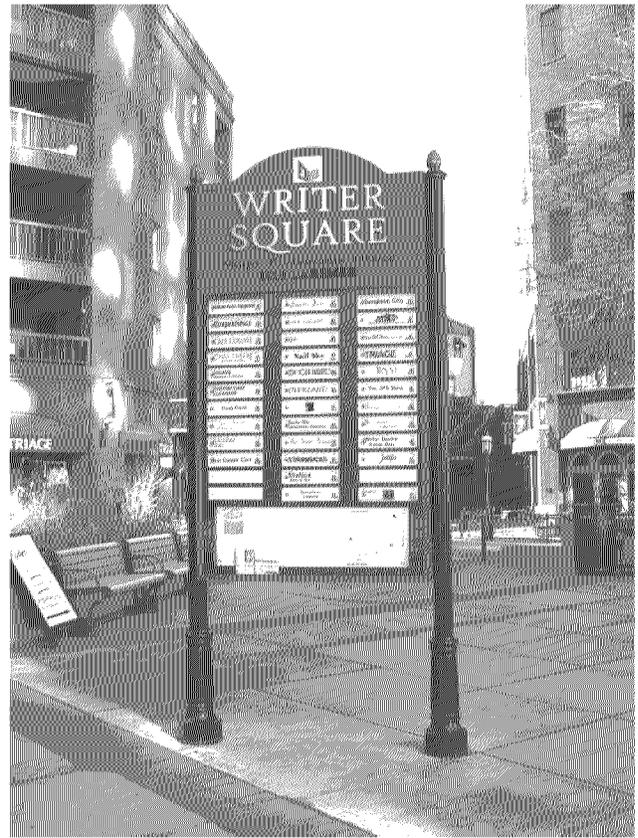
Unified street signs consist of street name with logo. It is recommended that the City prepare a signage package for street names, logos, banners with attention to size of letter, colors, identity theme, and visibility.

Way finding signs in an urban environment create place markers in minds of people. Such signage appears to provide identity as well as information. It may include name of the district, names of tenants, and a map with brochure among other design elements.

Business signage further reinforces the informational element on the Boulevard. These have been discussed in Chapter 4.0 Architectural Design Guidelines.



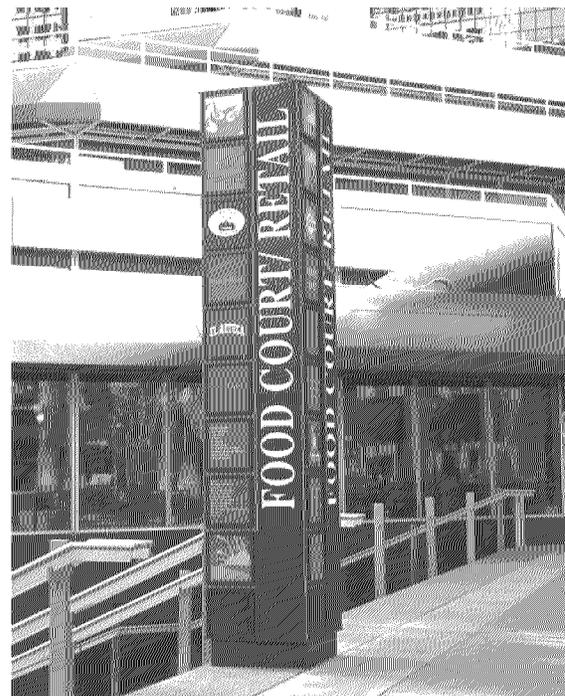
This clock post is designed to blend in with the rest of the streetscape style at the pedestrian level.



Downtown Denver



Existing Signage at Plaza Mexico



Downtown Denver

5.5 DEVELOPMENT STANDARDS

The site design and landscape development objectives for the Long Beach Boulevard Specific Plan area are:

- The on-site walkways will connect to the public sidewalks along bisected cross streets.
- Provide for off-street courtyards and plazas accessible from major pedestrian walkways, each with distinctive themes.
- Reserve sufficient area to create interesting spaces incorporating seating, lighting, interesting furnishings and decorative paving.
- Establish a series of focal points that create identity while extending the legacy of the City of Lynwood.
- Provide reduced setbacks commensurate with an urban setting.
- Limit the number of access points for automobiles into parking areas.
- Maximize the number of pedestrian openings without compromising the security of the residents.
- Promote pedestrian uses by conveniently locating access to the sidewalks.
- Provide a combination of patios, terraces and balconies to provide private open space.
- Provide sufficient areas between the garage and other buildings to create other amenities.
- Locate trash enclosures within the parking garage, screened from public and private views, decorative trash receptacles will be provided in active pedestrian areas.

6.0 ADMINISTRATION, IMPLEMENTATION, AND MAINTENANCE PLAN

6.1 INTRODUCTION

This chapter provides recommendations for the administrative, implementation, financing and regulatory methods to implement the Long Beach Boulevard Specific Plan for effective revitalization of the corridor. Further, it identifies maintenance measures for operations related to utilities, landscape, safety, and maintenance of all the villages on the Boulevard.

6.2 SPECIFIC PLAN ADMINISTRATION

The proceeding chapters of the Specific Plan have established the land use plan, building intensities and development standards, design guidelines, circulation and landscape plan. Subsequent to approval of the Specific Plan, implementation will entail review of various individual development projects. All developments within the Long Beach Boulevard Specific Plan are subject to the regulations and guidelines outlined in this document.

6.2.1 DEVELOPMENT REVIEW

To ensure that the policies, guidelines, and standards specified in this Specific Plan are met, projects within the Specific Plan area shall be subject to applicable City review and permits.

Such review may include, but not be limited to, site plan review, design review, conditional use permits, certificate of occupancy, and other applicable City procedures. The application requirements, review authority, required findings, conditions of approval, appeal procedures, and

revocation procedures shall be as specified in Chapter 25-Zoning procedures of City of Lynwood Municipal Code.

Applications for, as well as processing and approval of, proposed divisions of land within the Specific Plan Area shall comply with the provisions of the California Subdivision Map Act and Chapter 24- Subdivisions Map Act of the City of Lynwood Municipal Code.

6.2.2 GENERAL PLAN AMENDMENTS

City of Lynwood General Plan designated a variety of land uses within the Long Beach Boulevard Specific Plan area. These have been described previously, in Chapter 2.0 Existing Setting.

The adoption of this Specific Plan will apply the recommended land use designations as detailed in Chapter 3.0 Land Use Plan. General Plan amendments will be required to maintain consistency with the Specific Plan.

6.2.3 ZONING ORDINANCE AND PLAN AMENDMENTS

Existing zoning districts within the Specific Plan boundary shall be repealed and the zoning plan amended to indicate the adopted "Long Beach Boulevard Specific Plan." All development standards, design guidelines, and other development regulations of this Specific Plan shall apply. For the development standards and land use regulations that are not amended in this Specific Plan, the provisions within the Zoning Ordinance shall apply.

6.2.4 CONFLICT AND AMBIGUITY

Nothing in this Specific Plan shall be deemed to affect, annul, or abrogate any ordinances pertaining or applicable to the properties and their parcels affecting by the Specific Plan. If a conflict arises, the more restrictive requirement shall apply.

If ambiguity arises concerning appropriate classification of a particular use within the meaning and intent of this Specific Plan, or if ambiguity arises regarding matter of height, setbacks, landscape areas and other requirements, or zoning district boundaries, the remedies provided in the Zoning Ordinance for interpretations shall apply.

6.2.5 SUBSTANTIAL CONFORMANCE

The purpose of the following Substantial Conformance provision is to provide an administrative mechanism by which minor modifications to literal application of the Specific Plan or design departures from specific standards herein may be permitted by the City without amendment of the Specific Plan, provided that such departures do not result in significant impacts and are consistent with the intent, goals, and objectives of the Specific Plan. The ability to permit minor modifications to the Specific Plan via the Substantial Conformance provision allows for reasonable flexibility while maintaining the integrity of the Specific Plan, including protection against significant impacts to adjacent property owners. Substantial Conformance includes, but is not limited to, modifications necessary to comply with final conditions of approval or modifications affecting infrastructure, public services and facilities, landscape material and other issues except those affecting project financing and development regulations. Substantial Conformance include, but is not limited to, in-

clusion of land uses not listed in Chapter 3.0 of the Specific Plan; modifications that might be necessary to comply with final conditions of approval; or modifications affecting infrastructure, public services and facilities, landscape palette; and other issues. Except as otherwise provided below, Substantial Conformance shall not include significant modifications to the basic design of the project:

- a. Determination of Substantial Conformance shall be made by the City of Lynwood Planning Manager.
- b. Permitted Land Uses: Land uses specifically not listed as permitted in Chapter 3.0 of this Specific Plan may be permitted by the City, subject to a determination of Substantial Conformance, based on the following findings:
 - i. The proposed use is compatible with the permitted uses in the Planning Area within which it is proposed;
 - ii. The proposed use will not create any significant environmental impacts which were not previously addressed in the environmental document approved for the Long Beach Boulevard Specific Plan;
 - iii. The proposed use will not substantially increase the severity of any significant environmental impacts which were previously addressed in the environmental document approved for the Long Beach Boulevard Specific Plan; and
 - iv. The proposed use is similar to, and no more objectionable than the permitted uses in the designated planning area within which it is proposed.

- c. **Development Intensity:** The anticipated intensity of development within the Long Beach Boulevard Specific Plan is identified by each Village Plan in Chapter 3.0 Land Use Plan. The development intensity of any particular land use type may be exceeded pursuant to a Substantial Conformance approval by the City, provided that the overall development intensity of the Long Beach Boulevard Specific Plan is not exceeded, and that the exceedance of the development intensity for the particular planning area will not result in impacts greater than those anticipated in the adopted environmental document prepared for the Specific Plan.
- d. **Infrastructure:** Modifications to the alignment of roads, including resulting modifications to the alignment of the boundaries of individual parcels; creation of local public and/or private streets; or adjustments to individual infrastructure facilities such as drainage, sewer, and water may be approved by the City Engineer, provided that such modifications do not result in any significant impacts which were not previously addressed and resolved in the processing of the Specific Plan.
- e. **Design Details:** Modifications to design elements, such as paving treatments, wall materials, architectural details, landscape treatments and plant palette, sidewalks, lighting, entry treatments, and other details provided in Chapters 4.0 and 5.0 may be approved by the Planning Manager.
- f. **Land Use Approvals:** Specific provisions of land use approvals, architectural details; building size, height, bulk, and orientation; parking lot layout; and other site plan details may be permitted by the City to be revised utilizing substantial conformance provisions. In making such a determination, the Planning Manager shall be required to find that the revisions requested under Substantial Conformance are consistent with the provisions of the Specific Plan, and do not create impacts which were not recognized and addressed in the original approval.
- g. **Other:** Modifications in addition to those specifically enumerated above that are deemed to be minor by the Planning Manager, and which are in keeping with the intent of the Long Beach Boulevard Specific Plan and are consistent with the goals and objectives of the Specific Plan and City of Lynwood General Plan.

6.2.6 ADMINISTRATIVE MODIFICATIONS

Administrative modifications to the development standards of this specific plan of up to 10 percent may be approved, or conditionally approved by the Planning Manager upon demonstration that the proposed adjustment would enhance the overall appearance and function of the project; would be compatible with; and would not be detrimental to, adjacent properties or improvements; and would advance the goals of the Long Beach Boulevard Specific Plan.

6.2.7 AMENDMENTS TO THE SPECIFIC PLAN

This Specific Plan, or any portion, shall be amended or replaced using the same procedure as for the original Plan adoption.

6.2.8 ENFORCEMENTS AND PENALTIES

The penalties and procedures resulting from violation of the provisions of this Specific Plan shall be as listed in Chapter 25-Enforcements and Penalties (General Provisions) of the City of Lynwood Municipal Code.

6.3 IMPLEMENTATION

The implementation of Long Beach Boulevard Specific Plan will typically occur in two ways:

- 1) Incremental development following existing ownership patterns with consolidation of several parcels by developers; and/or
- 2) City-led initiatives for capital improvements to streets, sidewalks, and connections to transit and utilities.

Implementation of the Long Beach Boulevard Specific Plan will require property assembly of small lots, rezoning, and partial or complete redevelopment of existing land uses. Because the Specific Plan area is largely developed, phasing of new development and transitioning of commercial lands to mixed-use development should be accommodated in such a manner that would not severely interfere with the existing retail and commercial uses staying open for business.

The following planning actions or programs are recommended for effective implementation of the Specific Plan.

6.3.1 FACILITATE PARCEL ASSEMBLY

A mixed-use land use pattern is necessary to achieve the objectives of this Plan. The

City of Lynwood's Redevelopment Agency should assist in assembly of key parcels through development agreements or purchasing properties.

6.3.2 ECONOMIC RESTRUCTURING

This program focuses on identifying the existing economic base and recruiting new types of businesses depending on the market assessment. As the main two central villages of this Specific Plan are intended to function as downtown for the City, a "Main Street" approach is applicable. The main street program builds local leadership and commitment to care for the Boulevard on a permanent basis. The City should apply for recognition as a Main Street demonstration city, or it can implement the program on its initiative. Over a period of time, the Boulevard will become and function as the "town center." Additional information on the Main Street program can be obtained from the following organizations:

- The California Downtown Association
5857 North Temple City Boulevard
Temple City, California 91780
(818) 286-3101
- National Main Street Center
National Trust for Historic Preservation
1785 Massachusetts Avenue, N.W.
Washington, D.C. 20036
(202) 673-4219
- California Main Street
California Department of Commerce
1121 "L" Street, Suite 600
Sacramento, California 95814
(916) 322-1398

6.3.3 BUSINESS RECRUITMENT TEAM

As part of economic restructuring, a business recruitment team should be formed with members from the redevelopment agency, local chamber of commerce, and key merchants.

The team should identify details on issues related to business location, operations and marketing to support the revitalization of the corridor.

6.3.4 PROMOTION AND MARKETING PROGRAMS

The successful revitalization of Long Beach Boulevard in the City is predicated on effective promotion and marketing. Led by a downtown coordinator and in collaboration with the business recruitment team, specific marketing and promotional events and targeted business recruitment should be undertaken.

6.3.5 TOD COORDINATOR

Usually a downtown coordinator is designated with to devote energy and efforts for promoting the project.

In this instance, a TOD coordinator should be designated to specifically target transit oriented development potential, connections to the transit, parking, safety, and related issues. The coordinator would be responsible for facilitating the process of the downtown development, including accessing available transit oriented funding for City led initiatives.

6.3.6 BUSINESS IMPROVEMENT DISTRICT [BID]

The formation of a Business Improvement District (BID) is highly recommended to partly fund and maintain the revitalizations activities. SB-1424 allows cities to impose an assessment on business within self-designated downtown areas.

These self-assessed revenues can be used for improvements, such as the acquisition, construction, installation, or maintenance of any property with an estimated useful life of five years or more, including, but not limited to parking facilities, benches, trash receptacles, street lighting, decorations, banners, parks, fountains, and others. Revenues can also be used for activities, such as promotional events, security-personnel as well as camera installation and monitoring and maintenance of public spaces.

Additional information regarding the formation of BID can be obtained from the California Downtown Association.

- The California Downtown Association
5857 North Temple City Boulevard
Temple City, California 91780
(818) 286-3101

6.4 PUBLIC IMPROVEMENTS

The following Long Beach Boulevard public improvement projects are identified for implementation:

6.4.1 CIRCULATION

Improvements to the Boulevard involve implementing consistency along the length for the number of lanes, median and side-

walks. A striped bike lane addition will allow for safe bicycle movement. Bike parking "U" poles have to be added to side streets along the Boulevard in each village. Public works project for strengthening all the pedestrian crosswalks at key intersections with enhanced paving and installation of flashing lights is to be undertaken on the Boulevard.

Under-grounding utilities is recommended in the block north of I-105 in Village I near Tweedy Avenue.

Widening of the sidewalk is necessary for effective pedestrian and sidewalk design. Streetscape elements of landscaping, benches, planters, bus shelters along the Boulevard.

The City public works division along with the TOD coordinator will adopt a graphic package for street signs, way finding signs and gateway sign monuments.

6.5 POTENTIAL FUNDING MECHANISMS

This section summarizes the potential funding methods for improvements listed in the preceding section as well identify funding mechanisms for investment from public and private sector for developments along the Boulevard.

6.5.1 REDEVELOPMENT

The City's Redevelopment Agency is authorized to finance projects with property tax increment funds, interest income, Agency bonds, donations, loans from property, participation in development, sales tax as advanced or paid to the Agency in accordance with applicable pro-

visions of the law, or with financial assistance from the City, County, State of California, Federal Government, or any other public agency, or any other legally available source. Details for these financing methods can be found in the Redevelopment Plan for Redevelopment Project Area "A" for the City of Lynwood.

Redevelopment Agency has adopted the developer relation guidelines. These should be modified to include Design Review Board along with the landscape and lighting assessment district fees and application guidelines.

6.5.2 LANDSCAPING AND LIGHTING ASSESSMENT DISTRICT

Landscaping and lighting assessment district could be used for installation and maintenance of landscaping and lighting, through annual assessments on benefiting properties. This district may also provide for construction and maintenance of appurtenant features, including curbs, gutters, walls, fountains, planters, sidewalks or paving, and irrigation or drainage facilities.

City of Lynwood's landscaping and lighting assessment district is monitored by the Public Works Department.

6.5.3 COMMUNITY DEVELOPMENT BLOCK GRANT FUND

This program is already being utilized by the City of Lynwood for various projects, including creating jobs, removal of physical barriers to the handicapped, code enforcement, residential and commercial rehabilitation, down payment assistance, street improvements, youth and senior programs, and homeless services.

6.5.4 SOUTHERN CALIFORNIA EDISON UNDER-GROUNDING FUNDS

Utility companies are required to budget funds each year for under-grounding. These budgets are approved by the Public Utilities Commission and assigned to specific projects in each area based on priorities developed by local government.

City of Lynwood directs funding received for under-grounding utilities via the Lynwood Utility Authority. Currently the City has identified funds in the range of 2.4 million dollars for this purpose.

6.5.5 ECONOMIC DEVELOPMENT AGENCY [EDA] LOANS AND GRANTS

The Economic Development Agency offers various loans and grants to spur economic activity. Grants are available for site preparation, construction of public works projects that demonstrate job creation potential. Individual enterprises that are unable to obtain finance through conventional means could be eligible for some EDA loans.

Most of these loans and grants require matching funds of varying proportions.

- **Short-Term Planning Grants.** Administered by the Economic Development Administration, these grants may be used to provide support for significant new economic development planning, policymaking and implementation efforts, and establish comprehensive economic development planning processes cooperatively with the state, the state political subdivisions, and economic development districts. Eligible activities include preparation and

maintenance of a continuous comprehensive economic development planning process, coordination of multi-jurisdictional planning efforts, and diversification of the local economic base and implementation of programs, projects and procedures designed to create and retain permanent jobs and increase incomes. EDA assistance is limited to 12 months and may be extended up to 36 months.

- **Technical Assistance Grant (TAG) Program.** Administered by the Economic Development Administration, this program helps fill the knowledge and information gaps that may prevent leaders in the public and nonprofit sectors in distressed areas from making optimal decisions on local economic development issues. Eligible activities include supporting feasibility studies on potential economic development projects, revitalization plans, economic development conferences and seminars, and establishment of geographic information systems for local planning and development purposes.

6.5.6 TOD FUNDING

The following funding mechanisms are Federal, State, local/regional, and private funding sources to finance transit-oriented developments. It should be noted that the following options are not exhaustive, and other available funding sources may also be available. Programs and grants are categorized under three categories: (1) Transportation and Bicycle/Pedestrian Improvements, (2) Mixed Use and Residential Development, and (3) Economic Development Planning.

1. *Transportation and Bicycle/Pedestrian Improvements:*

- **Community Based Transportation Planning (CBTP) Grant Program.** Administered by the California Department of Transportation, this program provides funds for transportation/land use planning projects that support livable community concepts. Projects must have a defined transportation objective, and address a deficiency, conflict, or opportunity in coordinating land use and transportation planning. Project proposals must include a comprehensive public participation process, and must demonstrate the implementation of this process throughout the project. Eligible activities include transit-oriented development, mixed-use development, pedestrian/bicycle/transit linkages, jobs/housing balance, reuse or infill/compact development and/or community/economic development.
- **Non-Motorized Pilot Program.** Available for communities to create comprehensive transportation networks for walking and bicycling.
- **Safe Routes to School Program.** Communities can use this fund to fix hazards and slow traffic on roads that serve schools, as well as to build pathways, bike lanes, and sidewalks near schools, while increasing safety through focused enforcement and education programs. Funding is distributed by the State DOT, and projects can be funded at 100 percent.
- **Surface Transportation Program.** Administered by the Federal Highway Administration and Federal Transit Administration, this program can be used for any project including bicycle transportation, pedestrian walkways, carpool facilities, research, and safety improvements. It includes a 10 percent set-aside for the Transportation Enhancements Program. Funding is distributed by the State DOT.
- **Transportation Enhancement Program.** Community-based program to initiate transportation projects that promote health, safety, economic development and community pride. This program is a 10 percent set-aside of the Surface Transportation Program mention above.
- **Regional Transportation Improvement Program (RTIP).** Administered by the Regional Transportation Agencies, Metropolitan Transportation Commissions, and the California Transportation Commission. This program funds regional transportation capital improvement projects. Eligible activities include rail extensions, upgraded transit stations and vehicles, parking structures at major transit stations, construction of carpool lanes, road rehabilitation, and other related uses.
- **State Transportation Improvement Program (STIP).** Administered by the California Department of Transportation. This program is a multiyear capital improvement program resource management document to assist

- the state and local entities to plan and implement transportation improvements and to utilize resources in a cost-effective manner. Eligible activities include improving local roads, improving public transit (including buses), intercity rail, pedestrian, bicycle and intermodal facilities, and environmental enhancement and mitigation.
- **Congestion Mitigation and Air Quality Program (CMAQ).** Communities can use this program to improve air quality problems by investing in less-polluting transportation alternatives, including public transit improvements, fare/fee subsidy programs, and bicycling and walking facilities. CMAQ funds come to the state, but most of this funding is programmed by Metropolitan Planning Organizations as part of a broader plan to reduce air pollution. Funding can be disbursed to non-profit and private entities. CMAQ provides 80 percent funding, with a 20 percent local match required in most cases.
 - **Transportation and Community and System Preservation (TCSP) Pilot Program.** Administered by the Federal Highway Administration, this program provides funds for planning and implementation grants, technical assistance and research to investigate and address the relationship between transportation; community and system preservation; and private sector-based initiatives. Eligible activities include transit-oriented development projects, traffic-calming measures, and projects to reduce the need for future infrastructure investments.
 - **Bicycle Transportation Account (BTA) Program.** Administered by the California Department of Transportation, this program is intended to provide funds for bicycle transportation, which is recognized as an important and low cost mode of public transportation. It provides funds to local agencies for projects that improve safety and convenience for bicycle commuters. Eligible activities include bicycle path, lane or route construction and maintenance, bike lockers, racks on transit vehicles, planning, and safety education. Eligible cities must have an adopted Bicycle Transportation Plan. Cities must provide a local match of at least 10 percent of the total project cost.
 - **Petroleum Violation Escrow Account (PVEA).** Administered by the California State Legislature, the court ordered refunds to the State for price overcharges on crude oil and refined petroleum products during a period of price control regulations. Eligible activities include projects that conserve energy (public transit, ridesharing), and projects that provide restitution to the public. These funds must act as a supplement to other funds already available for the proposed project.
2. *Mixed Use and Residential Development:*
- **American Communities Fund (ACF).** Administered by Fannie Mae, ACF leverages the resources of the financial industry, for-profit and nonprofit

developers, federal government, state and local governments to revitalize emerging neighborhoods and downtowns. Can be used for single-family and multifamily developments, and mixed-use and neighborhood retail developments. Investment vehicles include equity investments, debt financing, and historic tax credit investments.

- **Downtown Rebound Planning Grants Program.** Administered by the California Department of Housing and Community Development, this program grants funds for local planning for infill housing, adaptive reuse (conversion) of commercial and industrial space into residential units, and the development of other forms of high density downtown housing. Eligible activities include infill site inventories, infill development feasibility studies, strategic action plans to remove barriers and promote infill housing, mixed-use developments and transit corridor development, and updates of general plans and zoning ordinances. Grants may also be used for seismic and structural feasibility studies on candidate buildings for adaptive reuse.
- **CalHome Program.** Administered by the California Department of Housing and Community Development, these funds enable low- and very-low-income households to become or remain home-

owners. Eligible activities include predevelopment, site development, rehabilitation and new construction, and acquisition and rehabilitation.

- **Child Care Facilities Finance Program (CCFFP).** Administered by the California Department of Housing and Community Development, these funds provide loan guarantees and direct loans for the development and/or expansion of childcare and development facilities, and family childcare homes serving more than six children. Eligible activities include purchase, development, construction, expansion, or improvements of license childcare and child development facilities and related equipment and fixtures.
- **Brownfield Economic Development Initiative (BEDI).** Administered by Department of Housing and Urban Development, BEDI grants, in conjunction with HUD Section 108 loans, may be used to revitalize brownfields. Eligible activities include environmental cleanup of sites, acquisition of a brownfield property and conveyance to a private sector party at a discounted price, use as funding reserves and direct enhancement of the security of a Section 108 loan. Eligible applicants are Community Development Block Grant (CDBG) entitlement communities and CDBG non-entitlement communities.

- **Affordable Housing Clearinghouse.** Administered by the Affordable Housing Clearinghouse, this private program provides lines of credit, community development loans, and commercial loans. The Clearinghouse is a consortium of lenders that pool their funds. Eligible activities include acquisition, group homes, homeless shelters, infrastructure development, mobile home park purchase assistance, new rental and for-sale housing, preservation of affordable housing, rehab of apartments and owner-occupied housing, self-help housing, Single Room Occupancy (SRO) hotels, and transitional housing. Orange County is a priority area for funding.
- **HOME Investment Partnership Program.** This program is already being utilized by the City of Lynwood for various residential projects, including deferred loans for rehabilitation/home improvements, correcting code violations, down payment assistance, land acquisition for affordable housing, and providing housing for homeless men.

Project located in the Specific Plan area are required to construct infrastructure and other facilities as part of their project approval when such improvements are directly related to their project.

In lieu of this method of financing, the City Redevelopment Agency may amend the developer relation guidelines to include special assessment/fee district to fund infrastructure and facilities related to circulation, landscaping, security-capital and operational costs. The conditions of approval will also include detail design review for conformance to the appropriate architectural style.

6.5.7 CONDITIONS OF APPROVAL FOR INDIVIDUAL PROJECTS

The current approval process includes development impact fees schedule that have been adopted by the Council and effective as of July 2, 2006, listed in Table 6.A. Further, the Redevelopment Agency has adopted the developer relation guidelines.

Table 6.A - Current Schedule of Development Impact Fees*

Facility Category	Single Family Fee per Dwelling Unit	Multifamily	Commercial Fee per 1,000	Industrial Building	Office Square Feet
Police Services	\$413.32	\$413.32	\$29.63	\$12.52	\$35.39
Parks/Recreation	2,270.10	2,270.10	—	—	—
Civic Center/City Garage	145.70	145.70	10.44	4.41	12.47
Water Facilities	1,381.25	1,381.25	2,244.53	1,381.25	1,381.25
Wastewater Facilities	347.17	208.30	433.96	267.32	267.32
Noise-Monitoring System	0.42	0.42	0.03	0.01	0.03
Technological Enhancements	35.02	35.02	2.51	1.06	3.00
Childcare	—	—	7.14	3.02	8.53
Subtotal	\$4,592.98	\$4,454.11	\$2,728.24	\$1,669.59	\$1,707.99
Administration 5%	241.00	235.00	143.00	88.00	90.00
Total Public Facilities Development Impact Fee ("PFDIF")	\$4,834.00	\$4,689.00	\$2,871.00	\$1,758.00	\$1,798.00
Transportation Facilities	\$320.67	\$224.47	\$1,439.81	\$234.09	\$368.77
Administration 5%	\$17.00	\$12.00	\$76.00	\$12.00	\$19.00
Total Transportation Development Impact Fee ("TDIF")	\$338.00	\$236.00	\$1,516.00	\$246.00	\$388.00
TOTAL	\$5,172.00	\$4,925.00	\$4,387.00	\$2,004.00	\$2,186.00

* Effective July 2, 2006

APPENDIX A: GLOSSARY

A.1 GLOSSARY OF ARCHITECTURAL AND PLANNING TERMS¹

Arcade: An arched roof or covered passageway.

Arch: A curved structure supporting its weight over an open space such as a door or window.

Architrave: The lintel extending from one column or pier to another, the lowest part of the entablature.

Attic Story: A story above the main entablature of a building.

Awning: A fixed cover, typically made of cloth over a metal armature, that is placed over windows or building openings as protection from the sun and rain.

Baluster: The upright portion of the row of supports for a porch railing.

Balustrade: A series of balusters surmounted by a rail.

Bargeboard: A finishing board at the edge of a gable roof.

Bay: A regularly repeated spatial element in a building defined by beams or ribs and their supports.

Bay Window: A window projecting outward from the main wall of a building.

Beveled Glass: Glass with a decorative edge cut on a slope to give the pane a faceted appearance.

Belt Course: A continuous horizontal band, either plain or ornate, which projects from the surface of an exterior wall, separating two stories. Ornate belt courses often resemble cornices.

Belvedere: A rooftop pavilion from which a vista can be enjoyed.

Big-Box Store: A very large retail commercial building such as a Wal-Mart.

Board and Batten: Vertical siding composed of wide boards that do not overlap and narrow strips, or battens, nailed over the spaces between the boards.

Bond: The general method of overlapping the joints of successive courses of bricks or stones, thereby binding them together to form a wall or other surface. Different patterns may be formed by these joints (e.g., common bond, Flemish bond, English bond, herringbone bond).

Bowstring: A roof structural system composed of parallel trusses which resemble a bow with the string parallel to and nearest to the ground.

Bracket: A support element under overhangs

Canopy: A fixed, roof-like covering that extends from the building as protection from the sun and rain.

Cantilever: A projecting overhang or beam supported only at one end.

¹ Some definitions were adapted from the Penguin Dictionary of Architecture by John Fleming, Hugh Honour, and Nikolaus Pevsner, Fourth Edition, 1991.

Capital: The upper part of a column, pilaster, or pier: the three most commonly used types are Corinthian, Doric, and Ionic.

Casement Window: Window with hinges to the side and a vertical opening either on the side or in the center.

Chamfer: A 90-degree corner cut to reduce it to two 45-degree edges. A bias cut.

Clapboards: Narrow, horizontal, overlapping wooden boards, usually thicker along the bottom edge, that form the outer skin of the walls of many wood frame houses. The horizontal lines of the overlaps generally are from four to six inches apart in older houses.

Clerestory: An upward extension of a single-storied space used to provide windows for lighting and ventilation.

Colonnade: A row of columns supporting a roof structure.

Column: A freestanding, upright member of a circular section, usually intended as a support.

Coping: The capping or top course of a wall, sometimes protecting the wall from weather.

Corbel: A type of bracket found in some cornices of brick buildings. It is formed by extending successive courses of brick so that they stand out from the wall surface.

Cornice: The continuing projecting section of an entablature at the top of a wall or, any projecting ornamental molding along the top of a building, wall, or arch, finishing or crowning it. That along the sloping sides of a pediment is

called raking cornice. The exterior trim of a building at the meeting of the roof and wall or projection at the top of a wall. A boxed cornice is a hollow cornice enclosing the eaves.

Course: In a masonry wall, a single line of bricks or stones.

Cupola: A lookout or similar small structure on the top of a building.

Curb Cuts: The elimination of a street curb to enable vehicles to cross sidewalks and enter driveways or parking lots.

Dentil: A small square shape often repeated in a horizontal line as an ornament in classical architecture.

Dormer: A vertically framed window which projects from a sloping roof and has a roof of its own.

Double Hung Window: A window with an upper and low sash arranged so that each slides vertically past the other.

Eaves: The overhang at the lower edge of the roof which usually projects out over the walls.

Eclectic: Stylistic classification based on historical periods is a common way to describe buildings, but the boundaries between stylistic periods are not always clear. Many buildings display characteristics of more than one style, resulting in eclectic hybrids often adapted to particular climates and cultures. Nevertheless, such categories are often a good starting point for understanding the visual properties of a building.

Elevation: A mechanically accurate, "head on" drawing of a face of a building or object, without any allowance for the

effect of the laws of perspective. Any measurement on an elevation will be in a fixed proportion, or scale, to the corresponding measurement on the real building.

Entablature: In classical architecture, the elaborated beam member carried by the columns, horizontally divided into architrave (below), frieze, and cornice (above). The proportions and detailing are different for each order, and strictly prescribed.

Façade: The exterior face of a building which is the architectural front, sometimes distinguished from other faces by elaboration of architectural or ornamental details.

Fanlight: Semi-circular window over a door or window with sash radiating like the ribs of an open fan.

Fascia: A flat strip or band with a small projection, often found near the roofline in a single-story building.

Fenestration: The arrangement and design of windows and doors in a building.

Finial: A formal decorative ornament at the top of a canopy, gable or pinnacle.

Flashing: Copper or other materials used to make weather-tight the joint between a chimney and a roof.

Flat Roof: A roof having only enough slope for drainage.

Fluting: shallow, concave grooves running vertically on the shaft of a column, pilaster or other surface.

Focal Point: A building, object, or natural element in a streetscene that stands out and serves as a point of focus, catching and holding the viewer's attention.

Frieze: The middle division of an entablature, between the architrave and the cornice, usually decorated but may be plain.

Gable: The triangular part of an exterior wall, created by the angle of a pitched roof.

Gable Roof: The triangular wall segments at the end of a double pitch or gable roof.

Gablet: A small ornamental gable.

Gambrel Roof: A roof with a broken slope creating two pitches between eaves and ridges, often found on barns.

Glazed Brick: A brick which has been glazed and fired on one side.

Glazing: Fitting glass into windows and doors.

Hip Roof: A roof with four uniformly pitched sides.

Historic District: A geographically defined area possessing a significant concentration or continuity of landmarks, improvements, or landscape features united by historic events or by physical development, and which area has been designated as an historic landmark district; said district may have within its boundaries noncontributing buildings or other structures that, while not of such historic and/or architectural significance to be designated as landmarks, nevertheless contribute to the overall visual character of the district.

Icon: A pictographic or graphic representation of an object. Used in signage to replace or supplement text.

Infill: A newly constructed building within an existing development area.

In-Kind Replacement: To replace a feature of a building with materials of the same characteristics such as material, texture color, etc.

Integrity: When a sufficient percentage of the structure dates from the period of significance. The majority of a building's structural system and materials should date from the period of significance and its character defining features should remain intact. These may include architectural details such as, porches, ornamental brackets and moldings and materials as well as the overall mass and form of the building.

Jamb: The vertical face of an archway, doorway or window.

Jobs/Housing Balance: The distribution of employment relative to the distribution of workers within a given geographic area. A geographic area is considered balanced when these distributions are approximately equal, and when available housing choices complement the earning potential of available jobs. When achieved, jobs-housing balance results in an adequate supply of housing (and therefore workers) being located within a reasonable distance of compatible employment opportunities.

Joist: Any small timber laid horizontally to support a floor or ceiling.

Keystone: The central stone of a true arch or rib vault.

Lintel: The horizontal member above a door or window which supports the wall above the opening.

Load-Bearing Construction: Construction in which walls, posts, columns or arcades support the weight of the ceilings and upper floors.

Loggia: A gallery behind an open arcade or colonnade.

Lot: A platted parcel of land intended to be separately owned, developed, and otherwise used as a unit.

Mansard: A roof with two slopes on each side, the lower slope being much steeper; frequently used to add an upper story.

Masonry: Wall construction of such material as stone, brick, and adobe.

Massing: A term used to describe the overall shape of a building, and how parts of a building relate to one another.

Mix Use Development: Three or more revenue producing land uses.

Mullions: The divisional pieces in a multi-paned window.

Muntin: A bar member supporting and separating panes of glass in a window or door.

Newel Post: The major upright support at the end of a stair railing or a guardrail at a landing.

Palladian Window: A three-part window with a top-arched center window and long, narrow rectangular windows on either side.

Panel: A sunken or raised portion of a door with a frame like border.

Parapet: A low wall placed to protect any spot where there is a sudden drop, for example at the edge of a bridge, quay, or house-top.

Pedestrian Pockets: A mix-use community that stresses walking and bicycling instead of parking lots and the automobile.

Pediment: A triangular piece of wall above the entablature, which fills in and supports the sloping roof.

Pier: A solid masonry support, as distinct from a column; the solid mass between doors, windows, and other openings in buildings.

Pilaster: A shallow pier or rectangular column projecting only slightly from a wall and, in classical architecture, conforming to one of the orders. A decorative feature which simulates an attached (“engaged”) pillar on a building. Used throughout history, pilasters were particularly popular for Art Deco buildings.

Pitch: The slope of a roof expressed in terms of a ratio of height to span.

Porch: An outside walking area having the floor elevated more than eight inches above grade.

Portal: The principal entry of a structure.

Porte Cochere: Carriage porch large enough to let a vehicle pass through.

Portico: A large porch, usually with a pedimented roof supported by columns.

Posts: The main verticals of walls or doorways that support a lintel.

Preservation: The act or process of applying measures to sustain the existing form, integrity and material of a building or structure and its site. This may include ongoing maintenance or stabilization work when necessary.

Primary Building Façade: The particular façade of a building which faces the street to which the address of the building pertains.

Purlin: A horizontal structural member parallel to the ridge, supporting the rafters. Can extend out from the gable.

Quoins: Heavy blocks, generally of stone, used at the corner of a building to reinforce masonry walls.

Rafter: A sloping structural member of the roof that extends from the ridge to the eaves and is used to support the roof deck, shingles, or other roof coverings.

Reconstruction: The art or process of reproducing by new construction the exact form and detail of a vanished building, structure or object or part thereof, as it appeared at a specific period of time.

Rehabilitation: The act or process of returning a property to a state of utility through repair or alteration which makes possible an efficient contemporary use while preserving those portions or features of the property which are significant to its historical architecture and cultural values.

Relief: Carving raised above a background plane, as in bas relief.

Restoration: The act or process of accurately recovering the form and details of a property and its setting as it appeared at a particular period and time by means of removal of later work or by the replacement of missing earlier work.

Reveal: The vertical side section of a doorway or window frame.

Ridge: The highest line of a roof when sloping planes intersect.

Rustication: A method of forming stonework with recessed joints and smooth or roughly textured block faces.

Sash: The part of the window frame in which the glass is set.

Section: A representation of a building, divided into two parts by a vertical plane so as to exhibit the construction of the building.

Setback: The minimum horizontal distance between the lot or property line and the nearest front, side, or rear line of the building (as the case may be), including terraces or any covered projection thereof, excluding steps.

Shake: Split wood shingles.

Shed Roof: A sloping, single planed roof as seen on a lean-to.

Sill: The exterior horizontal member on which a window frame rests.

Shiplap Siding: Early siding consisting of wide horizontal boards with "U" or "V" shaped grooves.

Slate: Thinly laminated rock, split for roofing, paving, etc.

Soffit: The finished underside of an eave.

Spandrel: The triangular space between the side of an arch, the horizontal above its apex, and the vertical of its springing; the surface between two arches in an arcade.

Stabilization: The act or process of applying measures designed to reestablish weather-resistant enclosure and the structural stability of an unsafe or deteriorated property while maintaining the essential form as it exists at present.

Storefront: The first floor area of a retail façade, emphasized by the display window or windows.

Street Frontage: The total linear dimension of all property lines which coincide with the edge of an adjoining street right-of-way.

Street Wall: The edges created by buildings and landscaping that enclose the street and create space.

Stringcourse: A narrow horizontal band extending across the façade of a building and in some instances encircling such decorative features as pillars or engaged columns; may be flush or projecting, and flat, molded or richly carved.

Stucco: An exterior finish, usually textured, composed of Portland cement, lime, and sand, which are mixed with water.

Terra-cotta: Earth-colored baked clay products formed into molds and used ornamentally. Also referred to roof tile color.

Traffic-Calming Devices: Speed bumps, traffic islands, rumble strips, etc. that

force drivers to slow down. Often used to keep drivers from speeding through neighborhoods or to make selected streets less attractive as alternative routes.

Traffic Circle: A circular one-way road at a junction of thoroughfares, facilitating an uninterrupted flow of traffic. Also called a roundabout.

Transit-Oriented Development (TOD): Development built around bus or rail lines.

Transom: The horizontal division or cross-bar in a window. A window opening above a door.

Turret: A little tower often at the corner of a building.

Urban Fabric: The generic term for the physical aspect of urbanism, emphasizing building types, thoroughfares, open space, frontages, and streetscapes.

Veranda: A roofed porch, sometimes stretching on two sides of a building.

Widow's Walk: A small roof deck with guardrail usually located at the peak of a roof from which wives of ship captains could catch a first glimpse of their husband's ship returning from sea.

A.2 GLOSSARY OF DESIGN TERMS

Aesthetics: The science and philosophy of beauty. If something is aesthetic, it is of beauty or artistic.

Asymmetry: The balanced arrangement of different elements without a common axis.

Balance: Another important aspect of rhythm. Balance can be described in terms of symmetrical and asymmetrical elements. An important feature of balance is that it is very often achieved by matching differing elements which, when perceived in whole, display balance.

Emphasis: Describes the use of elements which call attention to themselves. Emphasis is an important feature in creating balance when using dissimilar elements. Canopies and balconies are examples of elements which, when emphasized properly, can assist in presenting a balanced look. Emphasis also can be found within strip developments of malls by the location of a more massive or monumental building, such as a major department store. This emphasis provides a directional guide because it creates a point of reference for the users. Emphasis can also be used as a directional element such as the emphasis at a store entrance or mall entrance.

Garish: That which is gaudy, showy, flashing, dazzling, or too bright to be aesthetically pleasing.

Mass: Mass describes three dimensional forms, the simplest of which are cubes, boxes (or "rectangular solids"), cylinders, pyramids, and cones. Buildings are rarely one of these simple forms, but generally are composites of varying types of assets. This composition is generally described as the "massing" of forms in a building. During the design process, massing is one of many aspects of form considered by an architect or designer, and can be the result of both exterior and interior design concepts. Exterior massing can identify an entry, denote a stairway, or simply create a desirable form. Interior spaces (or lack of mass) can be designed to create an

intimate space or perhaps a monumental entry. Interior spaces create and affect exterior mass, and exterior mass can affect the interior space. Mass and massing are inevitably affected by their opposite, open space. The lack of mass, or creation of perceived open space, can significantly affect the character of a building. Architects often call attention to a lack of mass, by defining the open space with low walls or railings. Landscape architects also use massing in design such as in grouping of plants with different sizes and shapes. These areas are intended to be perceived as a whole rather than as individual trees or shrubs. Plant masses can be used to fill a space, define the boundary of an open area, or extend the perceived form of an architectural element.

Monochromatic: Painting with a single hue or color.

Monolithic: Exhibiting massive uniformity, singular.

Movement: The apparent directional emphasis of a building façade as indicated by its proportions. Static movement is based on square proportions; dynamic movement is based on rectangular proportions.

Pattern: The pattern of material can also add texture and can be used to add character, scale, and balance to a building. The lines of the many types of brick bonds are examples of how material can be placed in a pattern to create texture. The natural texture of rough wood shingles exhibit texture by the nature of the material and by the pattern in which the shingles are placed.

Proportion: Proportion deals with the ratio of dimension between elements. Proportion can describe height to height ratios, width to width ratios, width to height ratios, as well as ratios of massing. Landscaping can be used to establish a consistent rhythm along a streetscape which will disguise the lack of proportion in building size and placement.

Rhythm: The regular or harmonious recurrence of lines, shapes, forms, elements or colors, usually within a proportional system.

Scale: Scale is the measurement of the relationship of one object to another object. The scale of a building can be described in terms of its relationship to a human being. All of the components of a building also have a relationship to each other and to the building as a whole which is the "scale" of the components. Generally, the scale of the building components also relate to the scale of the entire building. The relationship of a building, or proportions of a building, to a human being is called its relationship to "human scale." The spectrum of relationships to human scale ranges from intimate to monumental. Intimate usually refers to small spaces or detail which is very much in keeping with the human scale, usually areas around eight to ten feet in size. These spaces feel intimate because of the relationship of a human being to the space. The distance of eight to ten feet is about the limit of sensory perception of communication between people including voice inclination and facial expression. This distance is also about the limit of an up-stretched arm reach for human beings, which is another measure of human scale. The components of a building with an intimate scale are often small and include details which

break those components into smaller units. At the other end of the spectrum, monumental scale is used to present a feeling of grandeur, security, timelessness, or spiritual well-being. Building types which commonly use the monumental scale to express these feelings are banks, churches, and civic buildings. The components of this scale also reflect this grandness, with oversized double door entries, 18-foot glass storefronts, or two-story columns. Landscape or hardscape elements can also bring human scale to a large building by introducing features such as a tree canopy, leaf textures, and fragrance. Plants can complement the scale of the architecture, as when large trees are used next to tall buildings, or small trees to accent a building component such as an entry.

Surface Materials: Can be used to create a texture for a building, from the roughness of stone or a ribbed metal screen to the smoothness of marble or glass. Some materials, such as wood, may be either rough (such as wood shingles or resawn lumber) or smooth (such as clapboard siding).

Symmetry: The balanced arrangement of equivalent elements about a common axis.

Texture: Texture refers to variations in the exterior façade and may be described in terms of the roughness of the surface material, the patterns inherent in the material or the patterns in which the material is placed. Texture and the lack of texture influence the mass, scale and rhythm of a building. Texture also can add intimate scale to large buildings by the use of small detailed patterns, such as brick masonry.